

March 27, 2017

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To: City Manager

From: General Manager Planning and Development

Subject: **Burquitlam-Lougheed Neighbourhood Plan – Phase 3 Summary**

For: **Council-in-Committee**

Recommendation:

That the Committee receive the report dated March 27, 2017 of the General Manager, Planning and Development entitled “Burquitlam-Lougheed Neighbourhood Plan – Phase 3 Summary” for information.

Report Purpose:

This report presents the results of Phase 3 of the Burquitlam-Lougheed Neighbourhood Plan (BLNP or “the Plan”) public consultation process, along with the proposed revised Plan and updated Development Permit Guidelines for Committee’s review and feedback in advance of presenting the final draft Plan to a future regular Council meeting as part of an Official Community Plan (OCP) amendment bylaw to add the BLNP to the City’s OCP.

Strategic Goal:

The Burquitlam-Lougheed Neighbourhood Plan (BLNP) process supports the strategic goal of ‘Strengthening Neighbourhoods’.

Executive Summary:

The Phase 3 public consultation period on the draft BLNP was held in January and February 2017. During that phase staff engaged in over 2,000 participant interactions on the draft Plan and received a large volume of feedback. Based on the written feedback received, it was noted that approximately 60% of respondents are ‘Supportive’, while 20% of respondents indicated that they are ‘Opposed’ to the overall draft Plan and a further 20% are ‘Neutral’.

The Plan charts a renewed course for the future of Burquitlam-Lougheed and is the City’s ‘blueprint’ for guiding growth and investment in the neighbourhood over the next 20-25 years while identifying the infrastructure, services and facilities necessary to support this growth.

Based on the level of public consensus that has developed around the proposed Plan, staff recommends proceeding with an OCP amendment bylaw and associated supporting elements, including a public hearing, to implement the Plan.

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Background:

On December 12, 2016 the draft BLNP was presented to Council in advance of the Phase 3 public consultation. Once adopted into the City's OCP, the BLNP will guide growth and development over the next 20-25 years in this strategically located area of Coquitlam.

Phase 3 Public Consultation Program

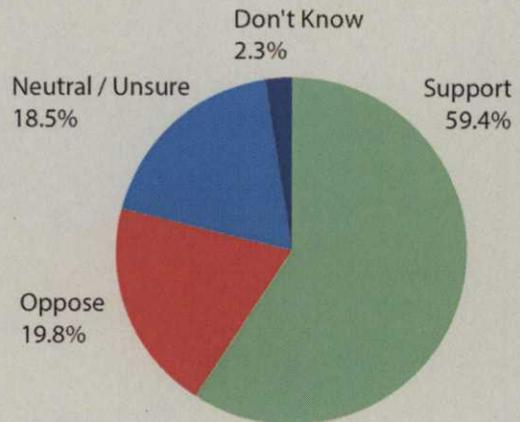
The Phase 3 public consultation program presented the draft Plan for public feedback to determine the level of community support. The project team sought input from a broad cross section of the public and stakeholders on the proposed Plan through an extensive public consultation and notification program. This resulted in over 2,000 direct participant interactions, through multiple components including three community information sessions, an on-line survey, the City's web site and social media, YouTube video, and meetings with community and stakeholder groups. Staff has also met with multiple residents and property owners, either in small groups or individually, to discuss the proposed Plan, listen to their views and answer questions.

The complete Phase 3 public consultation and notification program is described in Attachment 1. The following discussion summarizes Phase 3 feedback received and identifies major themes and proposed Plan refinements for Committee's consideration and feedback.

Discussion/Analysis:

Level of Support for Proposed Plan

Based on the written feedback received during the Phase 3 public consultation period there is a clear majority of Support (60%) for the overall draft Plan, with 20% Opposed and 20% Neutral/Unsure. A detailed analysis of all the Phase 3 feedback is summarized in Attachment 1.



Copies of all public feedback received are in the BLNP Consultation Summary binder in the Councilor's office and at the Planning and Development Front Counter.

Major Feedback Themes

Going beyond identifying the broader level of general support for the draft Plan, staff delved further into the feedback and identified a number of key themes, the top six include:

1. Increasing density overall (51 comments);
2. Increasing density in certain areas only (33 comments);
3. Reducing density overall (33 comments);
4. Providing fewer new streets (32 comments);
5. Providing fewer high-rise buildings (27 comments); and
6. Increasing affordable housing (26 comments).

Discussion/Analysis: cont'd/

Major Feedback Themes cont'd/

These themes reflect the diverse views in the community in determining the most appropriate manner to accommodate growth and address revitalization. It also indicates there is a growing level of acceptance for new, higher density development provided it is accompanied with improved housing choices and amenities.

Transportation and Parking Management Feedback and Approach

The proposed transportation and parking management approach was generally well received during the Phase 3 consultation phase with approximately 70% of the received feedback being 'Supportive' or 'Very Supportive' of the proposed overall transportation approach, 10% of respondents are 'Opposed' or 'Very Opposed' to it and the remaining 20% are "Neutral" to it.

Generally the community perception is that the SkyTrain and new high-density development will bring more vehicles to the area, and there is concern about the City's ability to effectively manage traffic and parking. Staff would note that the experience from other neighbourhoods with rapid transit stations and high-density development demonstrates that vehicle growth will be lower than what is perceived and are expected to be within the City's ability to manage as the area evolves. In addition, the street segments planned to be added to the current street network will help disperse traffic and improve multi-modal circulation.

Further analysis of the feedback found approximately 55% support for the proposed new streets approach and 50% for the parking management approach. Closer analysis of this feedback indicates that opposition to new streets and lanes is generally due to either personal property reasons or the concern that new streets and lanes will increase traffic and shortcutting through residential areas.

Engineering and Public Works staff will continue to monitor on-street parking in these neighbourhoods and will report back to Council as they refine and implement the parking management program.

A number of responses specifically mention opposition to the proposed extensions of Grover Avenue and Regan Avenue from Robinson Street to Gaultner Street. As discussed below under 'Resident Petitions', staff recommends removing the Grover and Regan street extensions and replacing them with a walkway/greenway at Regan Avenue in conjunction with other land use changes in this block. (Attachment 4, Schedule C)

Parks Feedback and Approach

The proposed parks approach had the highest level of support of any of the Plan components with 76% of respondents 'Supportive' or 'Very Supportive' while 7% of respondents were 'Opposed' or 'Very Opposed'. The written responses indicate that the public continues to rate the provision of improved parks and amenities as a high priority for the Plan area. However, feedback also indicates that residents and land owners are seeking greater clarity and certainty over the timing for improved parks and amenities.

Discussion/Analysis: cont'd/

Parks Feedback and Approach cont'd/

Based on this feedback, staff recommends proceeding with the majority of the parks approach in the draft Plan. However, it is also recommended that a decision on the proposed Kemsley/Clarke Park be deferred until the proposed future Oakdale local area planning process is undertaken, as further discussed below.

Staff has also worked to ensure the Coquitlam Family YMCA project and BLNP land use directions are coordinated to support the delivery of this major amenity.

Proposed Kemsley/Clarke Park

A number of written responses indicate opposition to the proposed new park at Kemsley Avenue and Clarke Road in Oakdale and express a number of potential operational concerns with a park in this location. Noting that the Oakdale area is proposed as a 'Future Planning Area', staff recommends that the proposed Kemsley/Clarke Park be removed from the BLNP land use schedule at this time and its 0.8 hectares (1.5 acres) is assigned to the existing unallocated 'Future Burquitlam Parkland' category in Section 3.3.1 of the Plan.

The ultimate location for park expansion in Oakdale is best determined in the future through the proposed Oakdale local area planning process in conjunction with consideration of other land use changes. That process will seek to build consensus in Oakdale as to the appropriate level of land use change, parks and amenities, and transportation improvements. Until that process occurs the majority of the Kemsley/Clarke Park site will remain as *One-Family Residential*.

A small portion of the Kemsley/Clarke Park site is proposed to be designated *Civic and Major Institutional* due to its utility as part of the SkyTrain guideway and tunnel infrastructure and the inability to develop structures over the tunnel portal. This would match the proposed *Civic and Major Institutional* land use for the rest of the at-grade portion of the guideway along Clarke Road south to Como Lake Avenue.

It should be noted that deferring the proposed Kemsley/Clarke Park will have park and recreation service implications beyond the Oakdale neighbourhood. In particular, there will be a service gap for residents on the east side of Clarke Road, in North Burquitlam, who will be beyond the target 10 minute walking distance to a park.

Resident Petitions: Proposed Land Use Concept Refinements

During the Phase 3 consultation phase, 10 petitions were received from groups of property owners advocating for or against a specific land use change in their area.

These requests have been evaluated and incorporated into the proposed Plan where they are found to be technically feasible, support the Plan goals, objectives and policies, fit with previous public feedback themes, support an improved transition between land uses, and fit with the neighbourhood context.

Discussion/Analysis: cont'd/

Resident Petitions: Proposed Land Use Concept Refinements cont'd/

Four requests were incorporated into the proposed Plan and six were not. Land use change requests that have been incorporated into the proposed Plan are shown on the map in Attachment 2 and full details regarding all the petitions are summarized in Attachment 3.

Revised Neighbourhood Plan

Building on feedback received from Council on December 12, 2016 and from the public during the Phase 3 consultation period, several revisions have been included in the proposed Plan (Attachment 4). These revisions seek to strengthen and clarify policy, correct spelling and grammatical errors, and incorporate the land use refinements described above. The revisions primarily focus on:

- Heritage conservation;
- Green streets;
- The Oakdale and Whiting Way/Appian Way Future Planning areas;
- Adaptable/accessible housing and rental housing;
- Day cares;
- Commercial frontage design; and
- Parks in the Oakdale neighbourhood.

For clarity, revisions to the proposed Plan are highlighted in blue text in Attachment 4.

Revised Heritage Policies

Following feedback from Council, staff has revised the heritage policies to encourage assessment of potential heritage buildings. While part of the BLNP area is covered by the Maillardville Heritage Inventory, the number of potential heritage properties in the BLNP is modest, with five properties included in the inventory (1 primary building, 1 secondary building, and 3 support buildings).

Based on this, and noting the location of these properties outside of the MNP area and its heritage conservation policies, a pragmatic policy approach is recommended to assess potential heritage properties and encourage their conservation through the use of Heritage Revitalization Agreements (HRAs). The upcoming Heritage Management Plan will further address this matter and develop a more refined policy approach.

Proposed Citywide Official Community Plan Amendments

In developing the BLNP, a number of new and innovative planning approaches have been developed that merit application elsewhere in Coquitlam. As such, there are four policy sections (Attachment 5) proposed as part of the draft BLNP that staff recommend be incorporated into the broader CWOCP so as to be utilized elsewhere in the City:

1. **Comprehensive Development (CD) zone criteria** – for unique or complex development sites, CD zones may be considered provided they meet specific evaluation criteria.

Discussion/Analysis: cont'd/

Proposed Citywide Official Community Plan Amendments cont'd/

2. **Plan amendment criteria** – recognizing that the OCP is a 'living document' intended to provide a flexible framework to guide development in Coquitlam over the coming decades, proposed evaluation criteria to consider future plan amendments have been included. This plan amendment evaluation tool will be applied to facilitate the review and processing of redevelopment and infill proposals that have recently been identified in Southwest Coquitlam.
3. **Comprehensive Master Plan policies** – Larger, complex, multi-phase development can benefit from undertaking a master plan process for redevelopment and provide important rationale, background and technical information to support their development application.
4. **SkyTrain guideway integration** – policies to require an appropriate design response for development adjacent to the guideway to ensure that safety, visual, and noise impacts are addressed.

These policy sections recognize the evolving nature of Coquitlam, expand on key learnings from the BLNP process, and have applicability beyond the BLNP area. These policy sections will also help to modernize the CWOCP and provide new tools for managing complex development applications across the City.

Proposed Development Permit Guidelines

The proposed Development Permit Guidelines (Attachment 6) build on the robust urban design framework in the proposed Plan and aim to ensure new development has a high-degree of architectural excellence, is highly walkable in all seasons, transit supportive, and enhances neighbourhood character with active ground-floors and human-scale design to frame streets and other open spaces.

The proposed Development Permit Guidelines will amend the existing Citywide Guidelines (Part 4 of the CWOCP) and will seek to implement the design vision for Burquitlam-Lougheed. For clarity, proposed revisions to the Development Permit Guidelines are highlighted in blue text.

Next Steps:

Recommendation to proceed with OCP Amendment Bylaw

Based on the above discussion and the revised BLNP as attached, staff recommends proceeding with the BLNP as an OCP amendment bylaw for Council's consideration at an upcoming regular Council meeting. This will be followed by a public hearing which will allow a final opportunity for public input. At that time, the BLNP will be supported by a Servicing Assessment, proposed Development Permit Guidelines, and related CWOCP and Zoning Bylaw amendments. These additional elements will help to ensure integrated implementation of the BLNP.

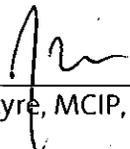
Financial Implications:

The BLNP is identified as a key part of the Planning and Development Department's 2017 Work Plan and is being undertaken with existing staff resources, as well as drawing on external consultant(s), as necessary, for strategic BLNP work components, and is funded within existing budgets.

Conclusion:

During the Phase 3 public consultation period, staff had over 2,000 participant interactions on the draft Plan and received a large volume of feedback. That feedback indicates approximately 60% support for the proposed BLNP. The Plan charts a renewed course for the future of Burquitlam-Lougheed and is the City's 'blueprint' for guiding growth and investment in the neighbourhood over the next 20-25 years, while identifying the infrastructure, services and facilities necessary to support this growth.

Based on the general level of public consensus that has developed around the proposed Plan, staff recommends proceeding with an OCP amendment bylaw and associated supporting elements, including a public hearing, to implement the Plan.



J.L. McIntyre, MCIP, RPP

AM/ss

Attachments:

1. BLNP – Phase 3 Consultation Feedback Summary (Doc. #2557342)
2. Recommended Land Use Changes (Doc. #2561236)
3. Petitions for land use change (Doc. #2564180)
4. Proposed Burquitlam-Lougheed Neighbourhood Plan (Doc. #2564138)
5. Proposed CWOCP Amendment (Doc. #2563093)
6. Draft Development Permit Guidelines (Doc. #2531441)

This report was prepared by Andrew Merrill, Major Project Planner and reviewed by Carl Johannsen, Manager Community Planning.

Burquitlam-Lougheed Neighbourhood Plan (BLNP) Phase 3 Public Consultation Feedback Summary Report

1.0 PHASE 3 OVERVIEW

The purpose of the Phase 3 consultation program for the Burquitlam-Lougheed Neighbourhood Plan (BLNP) was to present the draft land use concept and plan policy document to the community and gather feedback on the general level of support for the plan.

In addition to the recommended land use concept, the consultation process presented the proposed transportation, new streets and lanes, parks and amenities, and parking management approaches, for public review and feedback. The consultation events also served to raise general awareness about the planning process and provided community members the opportunity to dialogue with City staff.

The BLNP public consultation program was designed to receive input from a broad cross section of the public and stakeholders. Several outreach activities were undertaken to ensure that the public was engaged and well informed throughout the Phase 3 BLNP process. The primary consultation event consisted of three community information Open House sessions held on February 1, 11, and 15, 2017. One Open House event scheduled for February 8 was postponed to February 15 due to extreme weather conditions. Other means of communication and outreach were an online survey, the City's website, and social media, and presentations to community and stakeholders groups.

Staff engaged in over 2,000 participant interactions during this consultation phase and those inputs are the basis for the analysis and figures included in this report. Public feedback was primarily collected through comment forms distributed at the Open Houses and through the online survey on the project webpage. The consultation process provided community members with the opportunity to ask City staff clarifying questions and to express their opinions regarding the draft Land Use Concept and plan policies. The deadline for submission for paper comment forms was February 24 while online survey access closed on February 28, 2017.

2.0 CONSULTATION AND NOTIFICATION PROGRAM

The Planning and Development Department is continually working to broaden its approach for promoting public input opportunities and raise awareness about major planning projects. This includes utilizing a wide range of communication channels to attempt to reach a broad range of residents and stakeholders, such as Canada Post mail-out, newspaper ads, social media, e-mail, YouTube videos, neighbourhood signage, notifying community and stakeholder groups, plus the City and project webpages.

2.0 CONSULTATION AND NOTIFICATION PROGRAM cont'd/

2.1 Public Notification Methods

Direct Communication:

- Mail-out to all households and businesses within the plan area, plus a buffer zone outside, in advance of Community Information Sessions (10,207 in Phase 1; 12,143 in Phase 2; 13,734 in Phase 3).
- 6,961 door hangers delivered to every house to accompany the Tri-City News delivered the week of January 20, 2017.
- 26 project listserve e-mails (December 9, 2016, January 18, 2017, February 2 and 8, 2017 – 16 x 2) — 586 members as of March 10, 2017.
- Regular updates provided to 26 community and stakeholder organizations.

Web Based Notification:

- Plan document ISSUU statistics – 2,364 “reads” and 5,891 “impressions”. Average time spent – 6 minutes and 6 seconds.
- Social media posts — Facebook (8095 followers as of March 21) and Twitter (6,675 followers).
- Video 2 - New *BLNP A New Draft Plan* illustrated promotional video (uploaded February 1, 2017) - 493 views on YouTube + 1325 on Facebook = 1774 views (as of March 21).
- Video 1 – previous illustrated videos — over 5,400 views on YouTube and Facebook.
- Consultation events listed in the City's Events Calendar.
- City's main website — landing page image.
- Planning and Development Department webpage — landing page image.

Promotional Awareness (Phase 3):

- Three newspaper advertisements in Tri-City News (January 25 and 27 and February 3, 2017).
- Tri-City News article 'A building boom in West Coquitlam' on February 2, 2017.
- CBC Radio: The Early Edition interview – “Intersections: how will SkyTrain change Cottonwood and North Avenue in Coquitlam?”
- Other Language Media:
 - Vanchosun Media - ¼ page ad colour Wednesday, February 8 and Friday, February 10;
 - Shahrvand - ¼ page colour plus translation on Thursday, February 9;
 - Ming Pao - ¼ page colour plus translation on Thursday, February 9.
- Posters in City facilities.
- Large posters in Burquitlam SkyTrain Station (two platform ads + two concourse ads).
- Three neighbourhood signs located at Clarke and Robinson, Foster and Robinson, and Lougheed and Guilby.

BLNP Phase 3 Public Consultation Feedback Summary

2.0 CONSULTATION AND NOTIFICATION PROGRAM cont'd/**2.2 Public Consultation Program**

The Planning and Development Department is also continually working to improve its public consultation program in order to receive input from a broad cross-section of stakeholders. The public consultation process for BLNP includes multiple components including community information Open Houses, online surveys, the web and social media, YouTube videos, and meetings with community and stakeholder groups. Staff have also met with numerous residents, either in small groups or individually, to review the land use concept and policies, listen to specific concerns, and answer questions.

Specific consultation efforts and events include:

Event	Phase 1	Phase 2	Phase 3 (to date)
Community Information Open House	January 31, 2015 – 275 people February 4, 2015 – 145 people 131 comment forms	November 14, 2015 – 248 November 17, 2015 – 158 294 comment forms	February 1, 2017 – 271 February 11, 2017 - 269 February 15, 2017 – 130 365 comment forms
Online Survey	December 9, 2014 to February 27, 2015 - 217 submissions	October 23 – December 31, 2015 - 270 submissions	February 1 to 24, 2017 - 462 submissions
Public Advisory Group	October 23, 2014 April 22, 2015	June 16, 2015 November 25, 2015	February 2, 2017
Commercial Property Owners workshop		June 3, 2015 55 owners invited	-
Oakdale Neighbourhood Association meeting	September 18, 2014	November 12, 2015 June 23, 2016	March 9, 2017
Oakdale Town Hall		July 21, 2016 – 180 people 105 comment forms	-
Burquitlam Community Association meeting	October 2, 2014	November 5, 2015 September 8, 2016	April 5, 2017
Sustainability and Environmental Advisory Committee	September 24, 2014	November 29, 2016	-
Coquitlam Youth Council	-	-	February 23, 2017
Multiculturalism Advisory Committee	April 16, 2014	May 20, 2015 October 18, 2016	-
Strategic Investment Advisory Panel		October 19, 2016	-
City of Burnaby meeting	November 18, 2014	June 8, 2015 November 19, 2015	March 8, 2017
City of Port Moody meeting	Jan. 20, 2015		-
Letters and petitions	7 letters 1 petition – 74 signatures	63 letters 9 petitions – 500 signatures	13 letters 10 petitions received with 261 signatures
E-mails and phone calls	73 e-mails and phone calls	284 e-mails and phone calls	150 (as of March 21)

2.0 CONSULTATION AND NOTIFICATION PROGRAM cont'd/

2.3 *Community and Stakeholder Groups List*

The following groups/organizations receive regular e-mail updates, in addition to the e-mail list-serve.

- Austin Heights Business Improvement Association
- Blue Mountain Baptist Church
- Burquitlam Community Association
- Burquitlam Organic Garden Society
- City of Burnaby
- City of New Westminster
- City of Port Moody
- Como Lake Church of the Nazarene
- Coquitlam Chamber of Commerce
- Coquitlam Public Library
- Coquitlam Presbyterian Church
- Coquitlam Youth Council – West
- Dogwood Pavilion Seniors Society
- Fraser Health Authority – Healthy Built Environment Program
- Greater Vancouver Homebuilders' Association
- Metro Vancouver
- North Road Business Improvement Association
- Oakdale Neighbourhood Association
- Coquitlam RCMP
- School District No. 43
- School District No. 43 – Parent Advisory Council
- Seizan Buddhist Temple and Cultural Centre
- SHARE Family Service and Community Services
- S.U.C.C.E.S.S.
- TransLink
- Urban Development Institute (UDI)
- West Austin Neighbourhood Association

3.0 FEEDBACK SUMMARY

3.1 *Overview*

The Phase 3 questionnaire was composed of seven multiple choice questions and one open-ended question. Public feedback was analyzed to identify the level of support for the overall draft plan, along with the transportation, parking management, and parks approach. Data interpretation was structured in three different ways:

- Quantitative data in percentages of support,
- Theme analysis, and
- Geo-location of petition details in maps.

The open ended question allowed the public to express their views on the proposed land use changes. Additional details on the public comments are included below.

3.2 *Community Objectives*

The eleven evaluation criteria from Phase 2 were once again presented to survey respondents as criteria with which to evaluate the proposed plan. The criteria were grouped in four categories: General, Housing, Transportation and Amenities. The first question asked respondents to identify their top five criteria for a successful plan. Figure 1 below summarizes the dominant themes for each category. The five highest ranking criteria are:

1. Expand and improve parks and amenities (388 responses);
2. Encourage renewal in the BLNP area (335 responses);
3. Protect watercourses and environmentally sensitive areas (334 responses);
4. Design streetscapes to be safe, accessible, and comfortable, so they are people-friendly (317 responses);
5. Provide more housing choice for a diversity of families (286 responses).

3.2 **Community Objectives** cont'd/

Question - We would like to track how your priorities may have changed since Phase 2 of the planning process. Identify up to five measures of success that are most important to you.

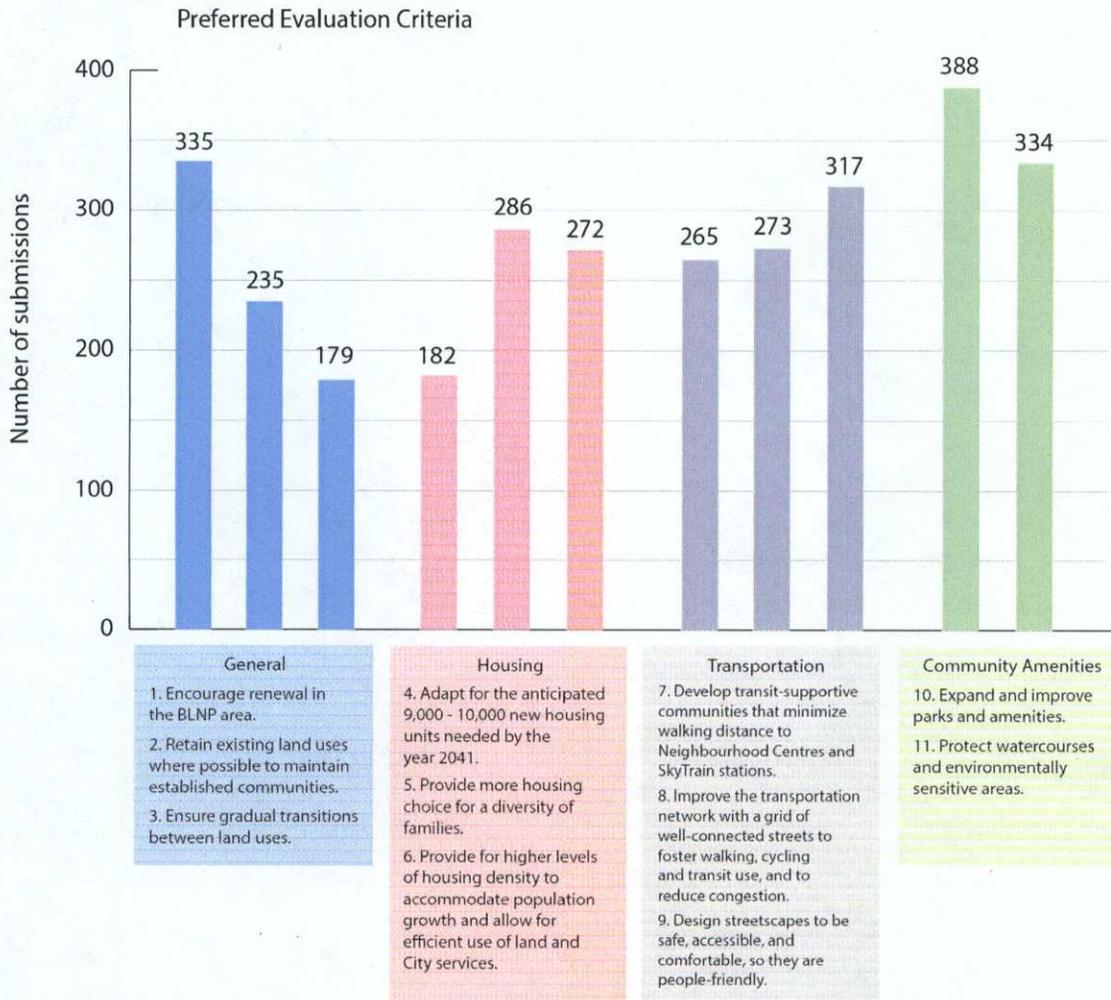


Figure 1: Measures of Success Criteria¹

Analysis – This question allowed respondents to communicate their priorities for their neighbourhood. It also helps us understand which issues are foremost in the public’s mind, and how priorities should be considered in weighing options that may appear to be in conflict.

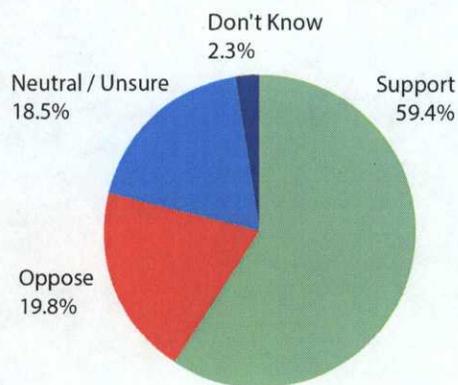
Similar to the Phase 2 results, this list reflects a diversity of opinion among respondents, who greatly value parks and amenities along with watercourse protection, while they see the importance of renewal of older neighbourhoods, and recognize the benefits of people-friendly streetscapes, along with improving available housing options.

¹ Results are based on combined data for the on-line surveys (n=462) and comment forms (n=365). Respondents had to choose 5 out of 11 criteria.

3.3 Community Support for the Draft Plan

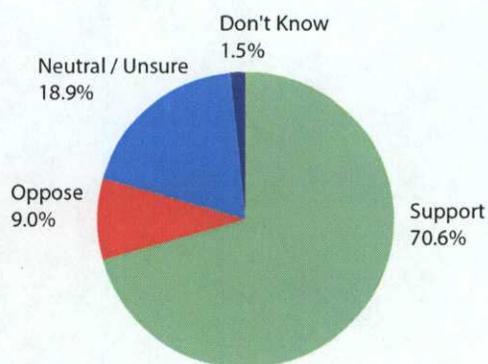
The heart of the questionnaire targets the level of support for the plan, by topic. Six response options were presented for each question: *Very Supportive*, *Supportive*, *Neutral/Unsure*, *Opposed*, *Very Opposed*, and *Don't Know*. For simplicity, the pie graphs below illustrate the combined *Very Supportive* and *Supportive* responses, as well as the combined *Very Opposed* and *Opposed* responses.

Question - What is your level of support for the overall draft plan?



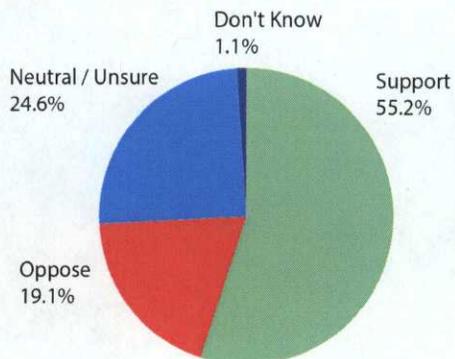
Analysis – A majority of respondents support the overall draft plan direction, however equal numbers are Opposed as are Neutral / Unsure. It is necessary to refer to the accompanying written responses to see the diversity of respondent concerns.

Question - What is your level of support for the sustainable transportation approach?



Analysis – A clear majority respondents are supportive of the sustainable transportation approach as presented in the Draft plan.

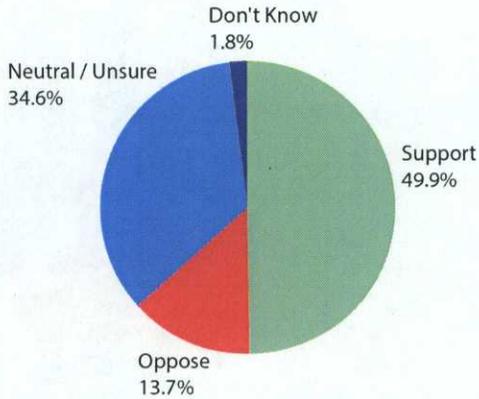
Question - What is your level of support for the approach to create new streets and lanes?



Analysis – While the majority is in support of the approach, more respondents are neutral or unsure, than those who are in opposition. Deeper analysis indicated that many of those opposed or neutral are due to concerns of increased shortcutting and personal property reasons.

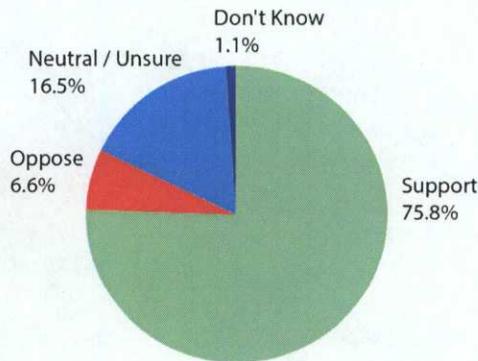
3.3 Community Support for the Draft Plan cont'd/

Question - What is your level of support for the parking management approach?



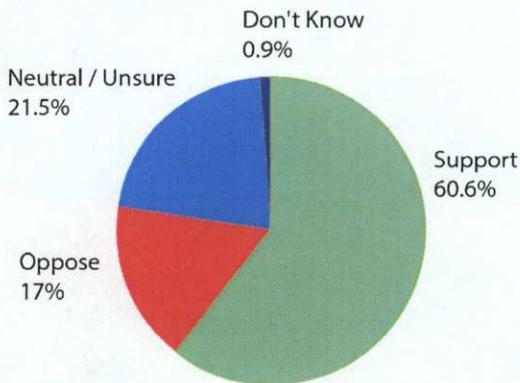
Analysis – While half of respondents support the direction, it is notable that nearly 35% are neutral or unsure. An analysis of written feedback indicates uncertainty about effective parking management.

Question - What is your level of support for the parks approach?



Analysis – A clear majority of respondents are in favour of the parks approach as presented in the draft plan. This reinforces the long-standing feedback for improved parks and amenities.

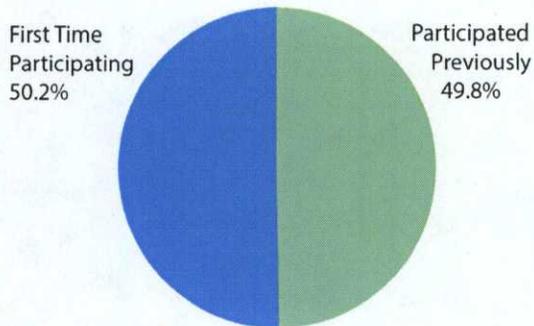
Question - What is your level of support for the housing directions in the draft plan?



Analysis – A clear majority support the housing direction proposed in the draft Plan, to provide a range of housing options to help meet the needs of a variety of family types.

3.3 Community Support for the Draft Plan cont'd/

Question - Did you participate in the Phase 1 or 2 consultation process? (i.e. Attended an information session, reviewed the plan or provided feedback).



Analysis – The response to this question reveals that only half of respondents had previously been involved in earlier consultation phases of the BLNP. This may indicate that more people are interested, or are getting involved in municipal planning efforts. It may also indicate the extent to which there has been turnover of interest throughout this multi-year neighbourhood planning process. It may also indicate the effectiveness of our communications efforts.

3.4 Theme Analysis

The majority of written responses referred to specific and localized issues such as particular developments, blocks, intersections or parks. However, a deeper analysis of the responses revealed a number of key themes.

Primary Themes:

These themes are mentioned most often in participant responses.

- **Increase Density Overall**
Many responses indicate support for increased density overall, the reasons given range from supporting redevelopment, providing more housing options, personal property reasons, refreshing the neighbourhood, and supporting the delivery of amenities and streetscape improvements.
- **Increase Density in Certain Areas Only**
The second most frequent theme references support for focusing increased density into specific areas, such as neighbourhood centres and along major roads.
- **Reduce Density Overall**
This theme is the inverse to the first theme, with respondents indicating a desire for less density, fewer new developments, with more preservation of existing neighbourhoods.
- **Fewer New Streets**
These respondents are concerned that traffic congestion will increase due to SkyTrain, and as new development brings more people to the neighbourhood. They are skeptical that people will switch to transit, walking and cycling, leading to more vehicle congestion. Deeper analysis indicated that many of those opposed are due to concerns of new streets leading to increased shortcutting and personal property reasons.

3.4 *Theme Analysis* cont'd/

Primary Themes cont'd/

- **Allow Fewer High-rise Buildings**
Similar to the 'reduce density overall' theme, some respondents expressed specific concern with high-rise buildings, specifically: design, shadowing, blocking views, traffic generation, change of character, small unit sizes, and the proposed height of high-rise buildings.
- **Increase Affordable Housing**
Affordable Housing constitutes a significant concern for the area. Respondents express their concerns about the lack of affordable housing options, especially for young families and low income people living in the area. Public feedback underlines the desire to locate more affordable housing close to areas well served by public transit.

Secondary themes:

Here are the next most common themes as mentioned in the feedback.

- **Increase Townhouse Options**
Respondents are generally interested in seeing more housing options in their neighbourhood to provide greater choice for a wide diversity of families. Increasing housing options for new families and long term residents in the area is seen as a priority. Accommodation for seniors and low income residents wanting to stay in the neighbourhood is also valued as important as new developments come in the area.
- **Increase Rental Housing Options**
Similar to the above, respondents are interested in seeing more rental housing options in their neighbourhood to provide greater choice for a wide diversity of families. Redevelopment on the site of existing rental buildings and displacement of existing rental tenants was also mentioned frequently.
- **Maintain Greenery and Trees**
Respondents value the parks and green open space in their neighbourhoods and they want to see them well-maintained and enhanced. Participant's feedback emphasizes the desire for retaining the existing mature trees as well as having new parks and green space. Burquitlam Park in particular is viewed as being in poor condition and residents want to see more public and green space around Burquitlam station.
- **Preserve Neighbourhoods**
There is some concern that the neighbourhood is changing too rapidly and some areas are losing their existing character. Some respondents indicated that redevelopment has proceeded far enough and should be stopped, to preserve existing neighbourhoods.
- **Increase Parking/Parking Management**
There is concern regarding on-street parking in residential areas close to the SkyTrain stations being used by commuters and new residents. Respondents are also interested in having Park and Ride facilities near the new SkyTrain stations.

3.4 Theme Analysis cont'd/

Figure 2 illustrates the most frequent common themes that respondents mentioned in their written feedback.

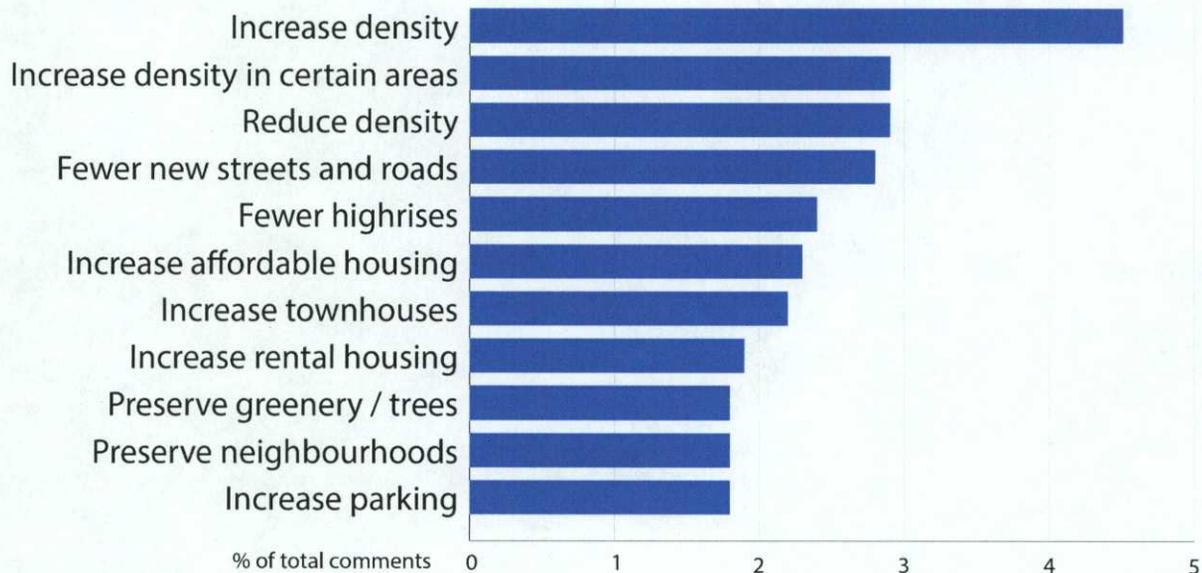


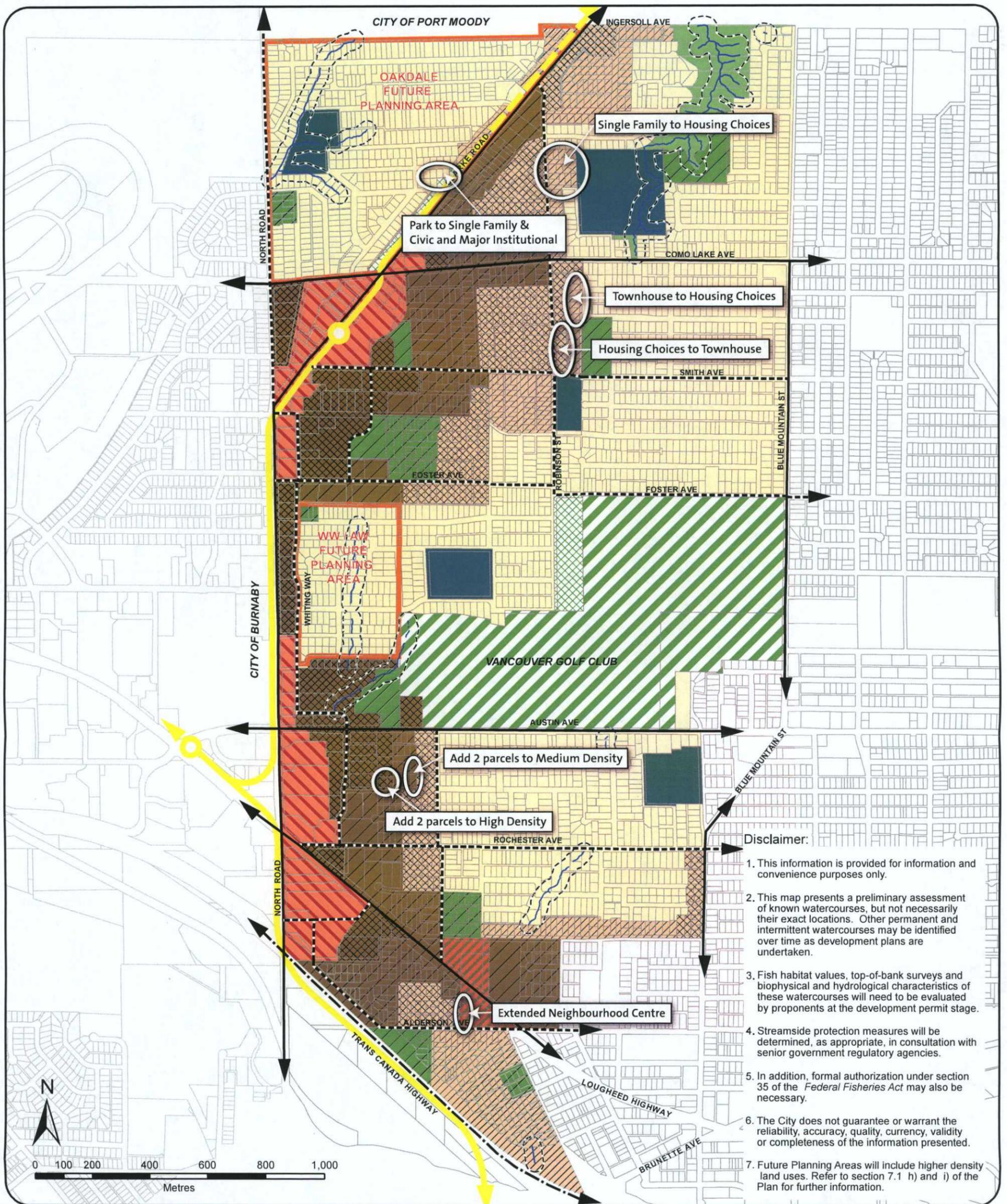
Figure 2: General Common Themes²

4.0 SUMMARY AND CONCLUSIONS

With over 2,000 direct participant interactions the high level of public participation and engagement demonstrates the community's high level of interest in the BLNP. There is a wide diversity of responses among the respondents and some polarization in the responses.

A clear majority (60%) supports the overall draft Plan and there is a general consensus of acceptance of higher density located in the "Core" areas, with varying levels of support for change in the "Shoulder" area. Generally the geo-location of responses shows an alignment with the proposed draft Plan. Analysis of written responses indicates that some of the most important issues to the Burquitlam-Lougheed community involve: increasing density overall, increasing density in certain areas only, reducing density overall, providing fewer new streets, providing fewer high-rise buildings, and increasing affordable housing.

² Results are based on combined data for the on-line surveys (n=270) and comment forms (n=294). Open question format analyzed for themes. Respondents may have identified more than one theme.



BURQUITLAM - LOUGHEED NEIGHBOURHOOD PLAN - LAND USE REFINEMENTS

LAND USE DESIGNATIONS

Transit Village Commercial	One Family Residential	Municipal Arterial Street
Neighbourhood Centre	Civic and Major Institutional	Municipal Collector Street
High Density Apartment Residential	School	Provincial Highway
Medium Density Apartment Residential	Parks and Recreation	SkyTrain
Low Density Apartment Residential	Natural Areas	SkyTrain Station
Townhousing	Extensive Recreation	Watercourse
Neighbourhood Attached Residential	Future Planning Area ⁶	Riparian Assessment Area (RAA)

DRAFT

Prepared By: Planning & Development
Source: City of Coquitlam - OCP GIS layer

Coquitlam

BURQUITLAM-LOUGHEED NEIGHBOURHOOD PLAN – PETITIONS FOR LAND USE CHANGE

During Phase 3 staff has received ten petitions from groups of property owners advocating for or against a specific land use change in their area.

These requests have been evaluated and incorporated into the proposed plan where they are found to be technically feasible, support the plan goals, objectives and policies, fit with previous public feedback themes, support an improved transition between land uses, and fit with the neighbourhood context.

In working through these refinements, the land use concept generally utilizes streets and parks/open spaces as the transition point between different land uses and densities, where possible, with the exception of cases where topography and historical development patterns require a transition at the lane or property line.

REQUESTS INCORPORATED INTO PLAN

The following land use change requests have been incorporated into the proposed Plan as indicated in Attachment 2, include:

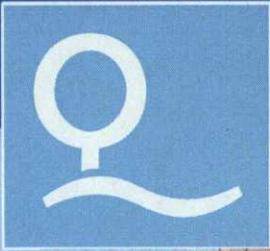
1. **Covina Court and the East Side of Robinson Street from Adiron Avenue to Egmont Avenue.** – (13 signatures). Change from *One-Family Residential* to *Neighbourhood Attached Residential* (NAR) supported due to a nearly unanimous request from residents within a distinct area that is adjacent to other NAR areas. The single-family areas to the north and east may be candidates for further NAR expansion, through the upcoming Housing Choices Review process;
2. **East Burquitlam: Guiltner, Townley, Grover, Regan and Cornell** – (142 signatures). Change the west side of Guiltner Street from *Townhouse* to *Neighbourhood Attached Residential*; and extend the *Townhouse* on Robinson Street to Smith Avenue. Staff also recommends removing the proposed street extensions at Grover and Regan and replace with a greenway walkway at Regan and improved rear lane alignments. The proposed walkway will facilitate the Regan Citywide Greenway.
3. **Central Lougheed: Sydney, Dansey, Madore and Rochester** – (34 signatures). Requested higher-density and the shifting of a proposed new local street. Following a review of potential street development scenarios, as well as typical medium-density development expected to occur in the area, staff made minor changes to simplify the border of the *High-Density* and *Medium-Density* areas, and the border of the *Medium-density* and *Townhouse* areas. The Central Lougheed area needs to incorporate an appropriate transition (including townhouses) to the existing single-family area east of Guilby Street (West Austin) where no change is proposed. Additionally, the location of the proposed extension of Clayton Street cannot be moved, as it needs to align with the existing portion of Clayton Street south of Rochester Avenue.
4. **633 Alderson Avenue and 632-633 Grayson Avenue** – These three lots are directly west of 688 Lougheed Highway which is proposed to be designated *Neighbourhood Centre*. The parcels are owned by the same individual who would prefer the properties be developed as one assembly and has requested for the *Neighbourhood Centre* designation to be extended to all properties. Staff agrees with extending the designation to facilitate this comprehensive development.

REQUESTS NOT INCORPORATED INTO PLAN

The following land use change requests have not been incorporated into the proposed plan as they do not support the plan goals, objectives, and policies; fit with previous public feedback themes; fit with the neighbourhood context; and/or are not technically feasible include:

1. **600 block of Fairview Street** – (Two petitions: four signatures for change and six against). In the draft BLNP the west side of Fairview is proposed to be *Townhouse* while the east side is proposed to remain *One-Family*. Four residents on the east side are requesting *Townhouse*, while six residents on both the east and west sides are requesting to remain *One-Family*. Given the interlaced nature of these requests, it is proposed that the east side remain as *One-Family* and the *Townhouse* designation remain on the west side as previously proposed in the draft Plan.
2. **600 Block of Robinson Street** – (11 signatures). Currently *One-Family* and requesting *Neighbourhood Attached Residential*. No changes are proposed as this request would 'leapfrog' over other *One-Family* areas to the west. Staff recommends that this request, along with the Fairview Street request described above, be more appropriately considered through the upcoming Housing Choices Review process.
3. **500 Blocks of Appian, Cochrane and Brookmere** – (10 signatures). This request is located within the proposed Whiting Way/Appian Way Future Planning Area and given it represents only 28% of properties in this area staff recommend that any future land use changes be considered through a future local area planning process that will seek to build consensus in this area as to the appropriate level of change.
4. **600 Block of Gardena** – (13 signatures). This request is located within the proposed Oakdale Future Planning Area and while a continuous block, it is located about 250 metres from the Clarke/Como Lake intersection and could involve 'leapfrogging' over single-family areas in between. Based on this, staff recommends that any future land use changes be considered through a future local area planning process that will seek to build consensus in this area as to the appropriate level of change.
5. **600 Blocks of Claremont and Tyndall and the 800 Block of North Road** – (27 signatures in favour, 10 opposed). This request is located within the proposed Oakdale Future Planning Area and does not show a continuous block of support, with pockets of opposition to change located within the petition area. Therefore staff recommends that any future land use changes be considered through the future Oakdale local area planning process as noted above.

Copies of all petitions and letters received are located in the BLNP Consultation summary binder in the Councillor's office.



City of Coquitlam

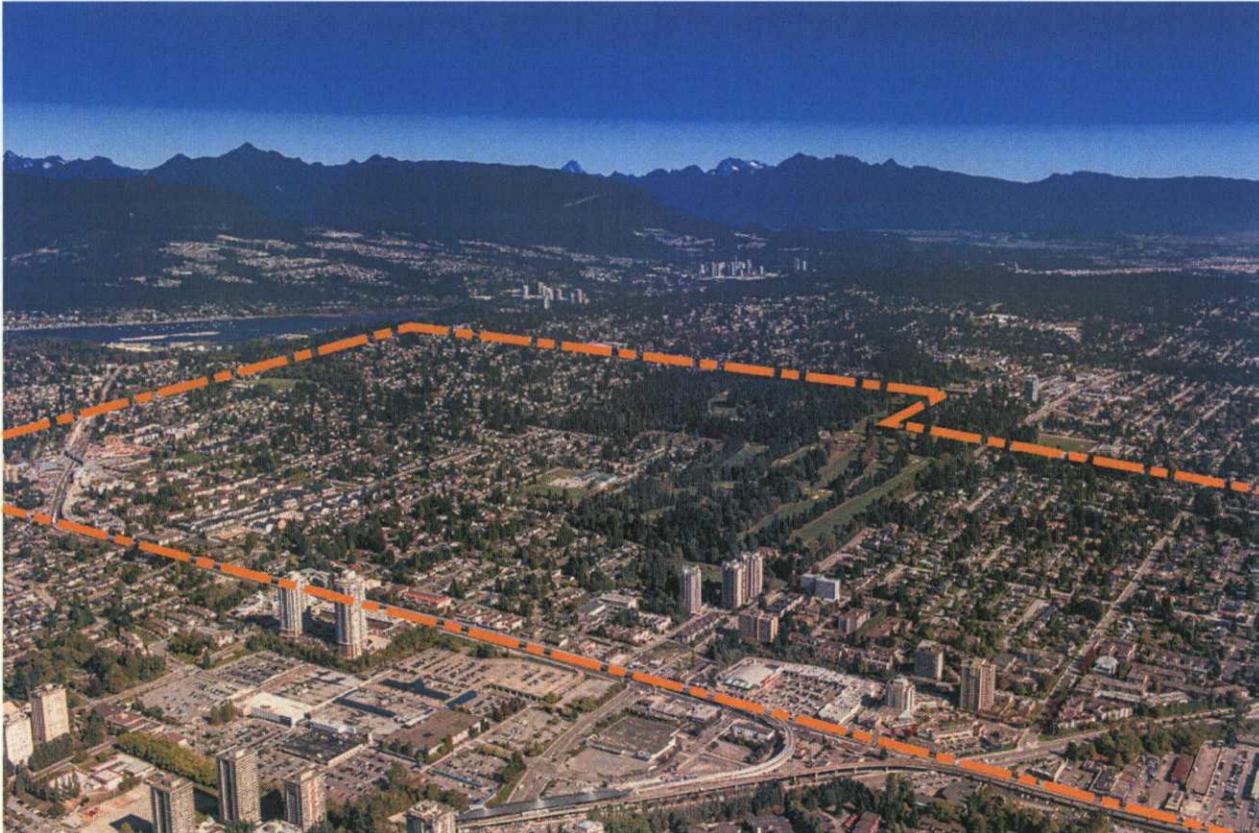
Citywide Official Community Plan - Chapter 9.2

Burquitlam-Lougheed Neighbourhood Plan

*Shaping growth in
a neighbourhood of
neighbourhoods*

Adopted XXX, X, 201X

Coquitlam
Planning &
Development



Through the use of community consultation, feedback from Coquitlam Council, and the application of transit oriented planning principles, this Neighbourhood Plan was developed by a multidisciplinary team of Coquitlam staff, led by Community Planning and including Development Planning, Engineering and Public Works, Parks, Recreation and Culture, Strategic Initiatives and Economic Development staff.

City of Coquitlam

Burquitlam-Lougheed Neighbourhood Plan

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	Schedule F - Districts	
	Schedule G - Watercourses and Riparian Areas	

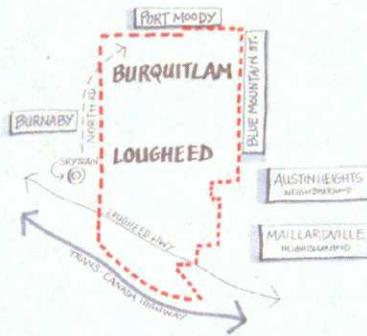


Figure 1: Burquitlam-Lougheed Neighbourhood Location

1.0 INTRODUCTION

1.1 Planning for the Future

The Burquitlam-Lougheed area, which is composed of a number of unique and established neighbourhoods, has a population of just over 20,000 people and approximately 370 businesses. Covering almost 560 hectares (1,400 acres) along the key North Road/Clarke Road corridor, the Burquitlam-Lougheed area connects with Burnaby to the west, Port Moody to the north, the Trans-Canada Highway to the south, and borders on the Austin Heights and Maillardville neighbourhoods to the east.



The arrival of the Evergreen SkyTrain extension into the Burquitlam-Lougheed area represents a major, regional transportation investment that will improve transportation access for residents and businesses. This new rapid transit service will also undoubtedly stimulate significant new growth along the SkyTrain corridor in the coming decades, which presents an opportunity to shape and focus this growth in a 'locally-appropriate' manner that maximizes housing choice, provides new amenities and recognizes existing neighbourhoods. Based on this, the City has updated and combined the existing Burquitlam and Lougheed Neighbourhood Plans to:

- Respond to change;
- Guide new housing and community growth;
- Plan for expanded, enhanced and new parks plus other civic facilities;
- Provide for housing and transportation choice;
- Build on the regional Municipal Town Centre and Frequent Transit Development Area designations to foster transit-supportive communities;
- Integrate new growth into existing established neighbourhoods;
- Maintain a high quality of life for existing and new residents; and
- Build great neighbourhoods.



This updated Burquitlam-Lougheed Neighbourhood Plan (BLNP) provides a vision for the future, as a plan that will guide growth for the next 20 to 25 years and accommodate **at least** 9,000-10,000 new dwelling units and over 15,000 new residents. Its primary aim is to ensure that new growth integrates with existing neighbourhoods, includes pedestrian-friendly and high-quality urban developments, recognizes the long-term growth potential of the North Road/Clarke Road corridor, and identifies the amenities, facilities, services and infrastructure needed to support growth over the long-term. **The BLNP is a living document that provides a flexible framework to guide growth and development over the next 25 years.**

1.1.1 Transit-Oriented Development

Transit-Oriented Development (TOD) is a pedestrian friendly, compact mixed-use form of development centered on **frequent** transit. This type of planning aims



to provide a more complete variety of uses, housing types and transportation alternatives to the automobile. Communities that are transit supportive are also more pedestrian and bicycle friendly; TODs can significantly influence overall travel patterns.

Transit-oriented neighbourhoods are often achieved through high-density, mixed-use developments with abundant ground-level retail, employment uses and a high-quality pedestrian-focused public realm and well-connected street and sidewalk networks. There are several TOD characteristics that are integrated into the Plan in order to foster a transit-supportive community:

-  Major **DESTINATIONS** and centres located along direct corridors making them easy to be served efficiently by frequent transit;
-  Walking **DISTANCE** to frequent transit is minimized by a fine-grained urban structure of well-connected streets;
-  People-friendly urban **DESIGN** including safe, comfortable, and direct pedestrian and cycling routes;
-  Higher residential and employment **DENSITY**;
-  Rich **DIVERSITY** of land uses and housing types;
-  Transportation **DEMAND** management measures that encourage walking, cycling and transit trips.

The Plan's vision, guiding principles, land uses, and policies seek to embed these characteristics throughout the plan.

1.2 Relationship to Other Plans

The Plan complements and further implements the policies of the *Citywide Official Community Plan (CWOCP)* as well as the *Southwest Coquitlam Area Plan (SWCAP)*. The updated BLNP replaces the previous Burquitlam and Lougheed Neighbourhood Plans, originally adopted in 2002.

The BLNP builds on the *City's Transit-Oriented Development Strategy (TDS)* and further implements the TDS vision. The BLNP vision, principles, land use concept and policies replace the TDS policies for this area.

For ease of use, this Neighbourhood Plan includes some cross-references in Table 1 to specific policies in the SWCAP. Other policies in both the CWOCP and SWCAP also still apply. However, if there is a conflict between a policy in the SWCAP or CWOCP and this Neighbourhood Plan, the Neighbourhood Plan policy takes precedence.

There are other key City plans, strategies, and bylaws that contributed to the policies contained in this Plan and build on one another to guide development in the BLNP area. These documents include the following, as amended from time to time:

- *Austin and Rochester Creeks Integrated Watershed Management Plan*
- *Burquitlam-Lougheed Servicing Assessment*
- *Burquitlam-Lougheed Streetscape Guidelines*
- *Burquitlam Station Area Transportation and Transit Integration Concept Plan*

- *Chines Integrated Watershed Management Plan*
- *Community Greenhouse Gas Reduction Strategy*
- *Housing Affordability Strategy*
- *Parks, Recreation, and Culture Master Plan*
- *Strategic Transportation Plan*
- *Pedestrian Wayfinding Plan*
- *Solid Waste Management Bylaw*
- *Stoney Creek Integrated Watershed Management Plan*
- *Subdivision and Development Servicing Bylaw*
- *Wildlife and Vector Control Bylaw*
- *Zoning Bylaw*

All of these plans, strategies and bylaws work in concert and support one another in order to guide growth and development in the Burquitlam-Lougheed area and throughout Coquitlam.

1.3 Plan Purpose and Structure

The purpose of this Plan is to chart a renewed course for the future of the Burquitlam-Lougheed area. This Plan is Council's 'blueprint' for guiding growth and investment in the neighbourhood. The Plan's vision, policies and implementation measures will help achieve the revitalization of the Burquitlam-Lougheed area.

The Plan was developed through a community planning process, involving more than 4000 participant interactions including consultation with Council, neighbourhood residents, property and business owners, the development community and City staff. Some readers will be seeking only a general understanding of what is planned for the Burquitlam-Lougheed neighbourhood while others will be looking for guidance and information on specific areas and aspects of the neighbourhood. With this in mind, the Plan is organized into seven key components:



- **Vision and Principles:** provides high-level direction that guide Plan policies;
- **Land Use Concept and Designations:** includes an 'at-a-glance' Burquitlam-Lougheed land use concept and associated land use designations (see Schedule A for detail);
- **General Policies:** contains land use, servicing and other policies that apply across the Neighbourhood;
- **Urban Design Framework:** provides policy direction to achieve a high quality public realm;
- **District Policies:** contains detailed policies for unique, specific locations within the Plan area, such as the Neighbourhood Centres;
- **Development Permit Guidelines:** directs readers to policies contained in Part 4 of the CWOCP that guide the form and character of commercial, multi-family residential, Housing Choices, and industrial developments; and that also guide the management of environmental areas; and
- **Implementation and Monitoring:** identifies 'next step' tasks that need to be completed to fully implement and monitor the Plan.



2.0 VISION, PRINCIPLES AND LAND USES

2.1 Plan Vision

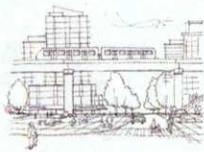
Developed in consultation with the Burquitlam-Lougheed community, and based on the community context and opportunities for growth and investment, this vision serves as the policy foundation for the Burquitlam-Lougheed Neighbourhood Plan:

Burquitlam-Lougheed is a complete community made up of a collection of distinctive neighbourhoods. These range from active neighbourhood centres and streets in the core, to quieter, outlying, lower-density residential areas.

Burquitlam-Lougheed offers a range of housing options, local employment, and social interaction opportunities, with high quality parks, streetscapes, and community amenities in a safe environment. Connected locally and regionally, people move efficiently by walking, cycling, transit, and driving.

2.2 Guiding Principles

These key principles help to implement the Plan vision and serve as the basis for Plan policies:



- a) **Revitalize Neighbourhood Centres** - Develop the Burquitlam and Lougheed Neighbourhood Centres as vibrant community hubs with activities and services that benefit the local community, and the broader City, by providing employment opportunities, shopping, services, and a variety of residential options in a walkable setting.
- b) **Improve Community Amenities** - Pursue strategies to improve and expand neighbourhood parks, open spaces, and community facilities and connect these to the Citywide network with walking and biking supportive infrastructure.
- c) **Increase Transportation Choice** - Integrate transportation with land use decision-making and support transportation accessibility and choice by providing greater connectivity to manage congestion, maximize potential transit ridership and make walking, cycling, and transit viable choices for all ages and abilities as the primary modes of travel.
- d) **Provide Housing Options** - Promote housing diversity to accommodate different types of households at a variety of life-stages including purpose-built rental, accessible/adaptable units, and foster housing choice balance between the Neighbourhood Centres and other areas.
- e) **Design on a Human Scale** - Incorporate human-scale building and site design principles with a focus on creating attractive buildings, high-quality public spaces and streetscapes within a safe, accessible, and pedestrian-friendly environment.
- f) **Foster Sustainability** - Demonstrate the importance of environmental, social, and economic sustainability in building and public realm design, community space, and policies that promote housing affordability, transportation choices, energy efficiency, supportive building densities, green and open space, and watercourse protection.

2.3 BLPN Land Use Concept

The Land use concept envisions Burquitlam and Lougheed as complete, transit-oriented neighbourhoods that contain a range of housing types in close proximity to shopping, employment and amenities set within a walkable, human-scaled setting with a high-degree of urban design excellence.

The plan seeks to accommodate at least 9,000-10,000 new dwelling units and an additional 60,000-65,000 m² (650,000 - 700,000 sq. ft.) of commercial floorspace that will be required to meet demand over the next 20-25 years. Key features of the Plan's land use concept include:

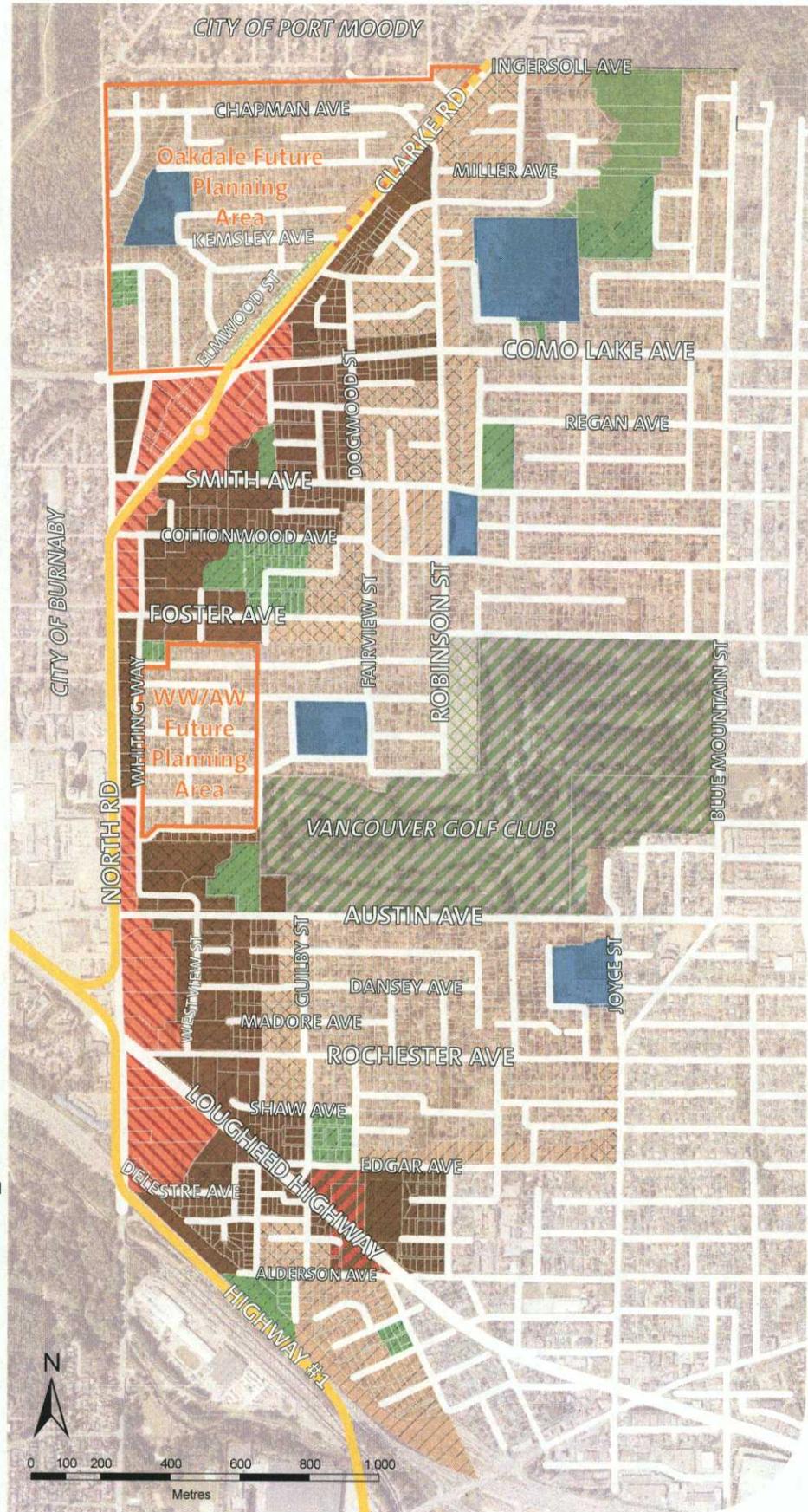


A community comprised of clean, safe, green and inclusive neighbourhoods that are connected to a convenient and affordable transportation network and vibrant commercial centres where residents can pursue education, recreation, sport and cultural interests that enhance their social well-being and strengthen their connection to each other and the community. Neighbourhoods that celebrate their uniqueness, history, heritage and character.

2016-2019 Strategic Plan

- Neighbourhood Centres and commercial areas near SkyTrain stations and along North and Clarke Roads. These areas will contain a variety of shops, services, offices, and amenities plus high-rise residential with top-quality urban design.
- A variety of residential neighbourhoods that contain a range of housing types to help meet the needs of different family types at different stages of life including high-rise apartments, low-rise apartments, townhouses, infill and single-family.
- The highest building densities are generally in the neighbourhood centres, and along North Road, and a transition to lower height and density as you move away from the centre.
- 10% of single-family housing has been reclassified to other uses, with approximately 42% of the area remaining as established single-family neighbourhoods where little change is expected in the short to medium term.
- A linked network of urban open spaces, plazas, squares, parks, and greenways to provide opportunities for resting, socializing, and programming of formal and informal leisure activities and active recreation.
- A multi-modal transportation network, enabled by land use changes, which encourages walking, cycling and transit use.





Adopted XXXX, X, 201X - Bylaw No. XXX, 201X

2.3.1 Land Use Designations

Land Use Designations are a classification that determines the type of existing or intended future use a property can have and describes the general intent, building types, and densities for different locations. Land Uses are essentially layered over the zoning that applies to properties, as described in the following section. The following Land Use Designations apply within the Burquitlam-Lougheed area and are illustrated on the land use concept (Schedule A).

These designations help to implement the Plan's policies and are based on the Land Use Designations in the *Southwest Coquitlam Area Plan* (SWCAP).

For further detail on Land Use Designations see SWCAP, Section 2.3, Policy CC1.



Transit Village Commercial - provides for the development of medium-and high-density Transit-Oriented Developments that accommodate residential uses, retail, commercial, professional services and a range of public amenities.



Neighbourhood Centre - provides for medium-density **Mixed-use** Development that accommodates residential, retail and commercial uses.



High Density Apartment Residential - accommodates apartment developments in tower forms in areas of high activity.



Medium Density Apartment Residential - accommodates townhouse and apartment developments at medium densities from three to eight storeys.



Low Density Apartment Residential - allows for multi-family residential use at lower densities up to two storeys.



Townhousing - provides a lower rise form of townhouse/**rowhouse** residential development, encouraging larger dwelling units suitable for family accommodation.



Neighbourhood Attached Residential - provides for innovative, small scale housing types, of up to four dwelling units, through the Housing Choices program that can be accommodated through a variety of building arrangements and flexibility in the distribution of floor area.



One-Family Residential - accommodates single-family residential homes on separate lots. Local commercial development is also accommodated, under specific circumstances, in this designation.



School - accommodates public and private educational institutions.



Civic and Major Institutional - accommodates civic and institutional buildings, sites and utilities.



Parks and Recreation - delineates those lands being used or intended to be used as public parks, oriented to formal and informal leisure activities, and active recreation.



Extensive Recreation - accommodates large outdoor recreational uses and limited associated facilities such as golf courses, marinas and sports clubs.



Natural Areas - delineates lands intended for protection and to remain in a natural state. "Natural Areas" provide habitat for wildlife and fisheries values and may include lands that have high environmental value and sensitivity, or are hazardous to development due to potential for land slippage, erosion, debris flows, or flooding. These lands may be used for passive recreation uses such as wildlife viewing, trails, and complementary minor structures.



2.3.2 Corresponding Zones

All land in the City is assigned a zone under the City's *Zoning Bylaw* (as updated from time to time). The zone on a property specifies the owner's development rights, including permitted uses, the dimensional requirements for lots, the height of buildings, setbacks from property boundaries. The zone also sets out the maximum floor area permitted on a property.

- a) The zones that may be applied to the Land Use Designations described in section 2.3.1 are specified in Table 1 below. For further detail on corresponding zones see the *Southwest Area Plan (SWCAP)*, Section 2.3, Policy CC2.

Table 1 – Corresponding Zones

Land Use	Corresponding Zone
Transit-Village Commercial	C-7 Transit Village Commercial RM-6 Multi-storey High Density Apartment Residential RM-5 Multi-storey High Density Apartment Residential P-1 Civic Institutional P-5 Special Park
Neighbourhood Centre	C-5 Neighbourhood Commercial RM-4 Multi-storey High Density Apartment Residential P-1 Civic Institutional P-5 Special Park
High Density Apartment Residential	RM-6 Multi-storey High Density Apartment Residential RM-5 Multi-storey High Density Apartment Residential RM-4 Multi-storey High Density Apartment Residential P-1 Civic Institutional P-5 Special Park
Medium Density Apartment Residential	RM-2 Three-Storey Medium-Density Apartment Residential RM-3 Multi-Storey Medium-Density Apartment Residential C-1 Local Commercial (in accordance with <i>SWCAP</i> Policy CC14) P-5 Special Park
Low Density Apartment Residential	RT-2 Townhouse Residential RM-1 Two-Storey Low-Density Apartment Residential C-1 Local Commercial (in accordance with <i>SWCAP</i> Policy CC14) P-5 Special Park
Townhousing	RT-2 Townhouse Residential RTM-1 Street-Oriented Village Home Residential RM-2 Three-Storey Medium-Density Apartment Residential (in accordance with Policy 3.2 i) C-1 Local Commercial (in accordance with <i>SWCAP</i> Policy CC14) P-2 Special Institutional (in accordance with <i>SWCAP</i> Policy CC29) P-4 Special Care Institutional (in accordance with <i>SWCAP</i> Policy CC30) P-5 Special Park

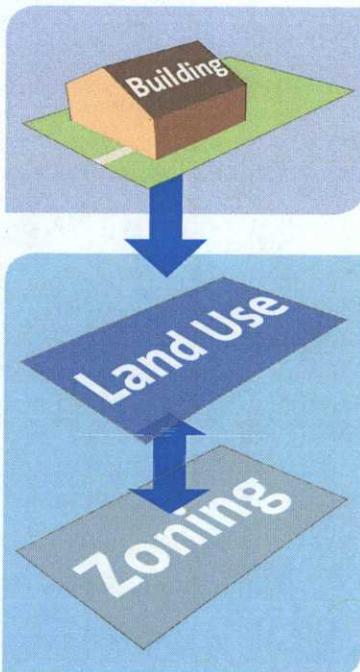


Figure 2: Land Use and Zoning work together to guide building form

9.2

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Land Use	Corresponding Zone
Neighbourhood Attached Residential	RS-1 One-Family Residential RT-1 Two-Family Residential RT-3 Triplex and Quadruplex Residential C-1 Local Commercial (in accordance with SWCAP Policy CC14) P-2 Special Institutional (in accordance with SWCAP Policy CC29) P-4 Special Care Institutional (in accordance with SWCAP Policy CC30) P-5 Special Park
One-Family Residential	RS-1 One-Family Residential RS-3 One Family Residential C-1 Local Commercial (in accordance with SWCAP Policy CC14) P-2 Special Institutional (in accordance with SWCAP Policy CC29) P-4 Special Care Institutional (in accordance with SWCAP Policy CC30) P-5 Special Park
School	P-1 Civic Institutional
Civic and Major Institutional	P-1 Civic Institutional P-2 Special Institutional (in accordance with SWCAP Policy CC29) P-5 Special Park
Parks and Recreation	P-1 Civic Institutional P-5 Special Park
Extensive Recreation	P-3 Special Recreational (in accordance with SWCAP Policy CC32) P-5 Special Park
Natural Areas	P-1 Civic Institutional P-5 Special Park

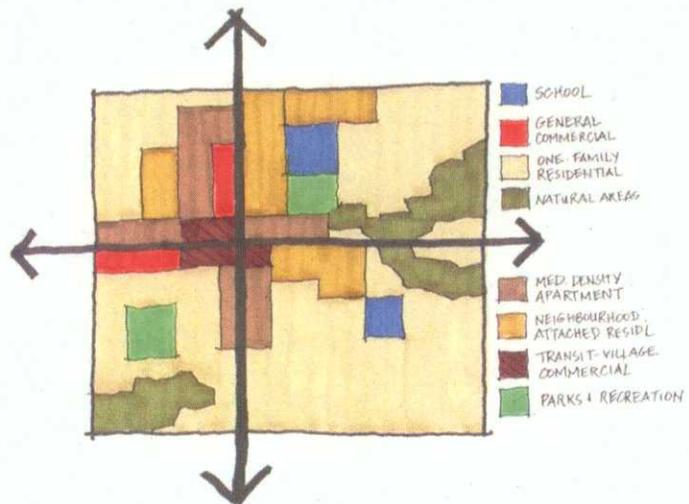




Figure 3: Floor Area Ratio (FAR) is used to measure density.

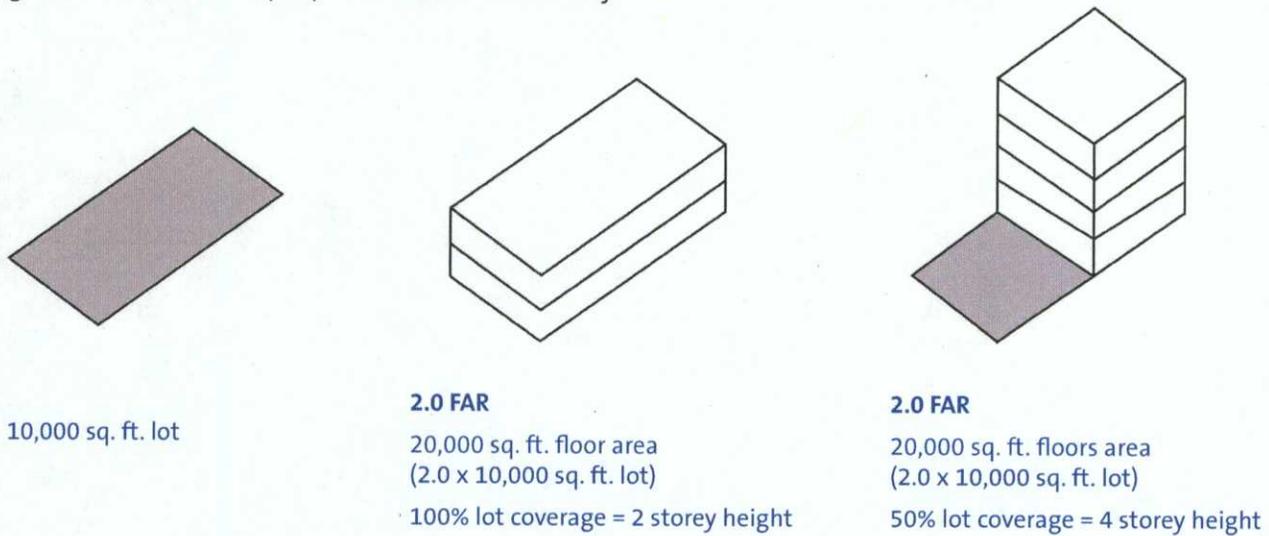
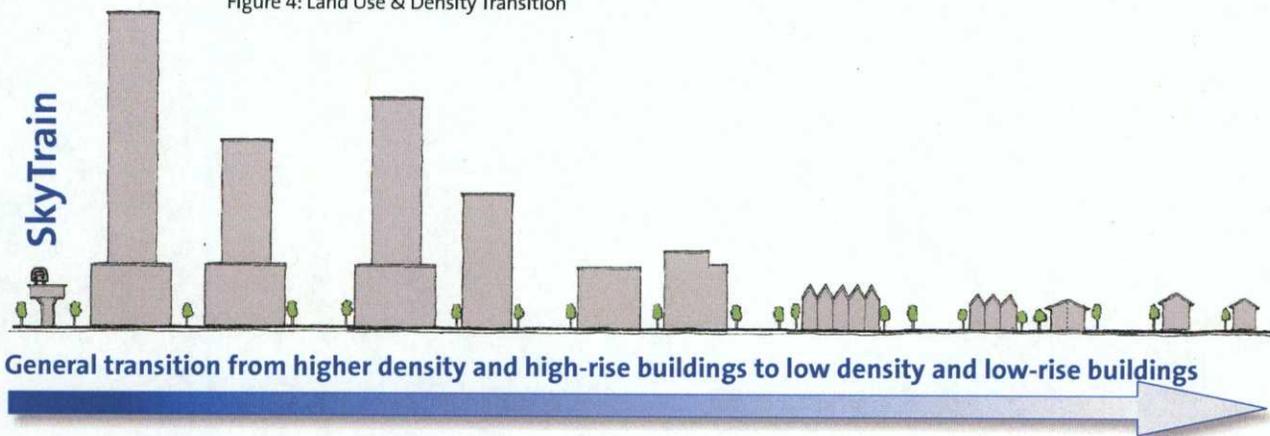


Figure 4: Land Use & Density Transition



Land Use Designation	Transit Village Commercial & High Density Apartment	High Density Apartment	Medium Density Apartment	Townhouse	Neighbourhood Attached Residential (Housing Choices)	One Family Residential
Zones Included	C-7, RM-6	RM-4, RM-5, RM-6	RM-2, RM-3	RTM-1, RT-2, RM-2	RT-1, RT-3	RS-1, RS-3
Maximum Density (FAR)	4.5 Floor Area Ratio (FAR)	3.0 - 4.0 FAR	2.4 FAR	0.9 - 1.4 FAR	0.75 FAR	18 units per hectare
Maximum Building Height	None; dependent on lot area and underground parking capacity	None; dependent on lot area and underground parking capacity	8 storeys (3-8 storey range)	2-3 storeys	11 metres (2 storeys)	11 metres (2 storeys)

Adopted XXXX, X, 201X - Bylaw No. XXX, 201X

3.0 GENERAL POLICIES

Based on the Plan vision, principles and land use concept, the following general policies apply throughout the Burquitlam-Lougheed neighbourhood.

3.1 Commercial

A key guiding principle of the Burquitlam-Lougheed Neighbourhood Plan is to **Revitalize Neighbourhood Centres** into vibrant, well-designed community hubs. The following policies, in conjunction with other plans, policies and strategies, aim to redevelop the Burquitlam and Lougheed Neighbourhood centres and the North/Clarke Road corridor with activities, shops, and services that benefit the local community by providing employment opportunities, shopping, and a variety of apartment options in a walkable setting.



Public Realm refers to all those parts of the built environment which allow public access. It encompasses: all streets, squares and other rights of way, open spaces and parks; and semi-public spaces.



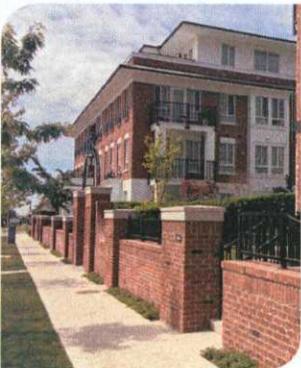
Policies:

- a) Require all new commercial development to front buildings onto streets, with active and transparent ground-floor frontages, and include high-quality, pedestrian-friendly and accessible site and building design. Blank walls and spandrel/opaque building walls facing streets, plazas and other public realm areas are strongly discouraged.
- b) Design the public-realm in a 'Main Street' format with continuous commercial frontages and weather protection to encourage walking. Allow for breaks in the facades to encourage a variety of business types and sizes and to accommodate small plazas or squares surrounded by active commercial uses and facilitate access to adjacent streets.
- c) Design frontages with smaller width storefronts to encourage variety and interest.
- d) Parking is to be concealed with no impacts to the space between the street and the business front.
- e) Pursue economic development opportunities, through advocacy and a supportive business climate, that promote new commercial uses including a mix of local-serving retail shops, anchor stores (e.g., grocery store, pharmacy), restaurants, hotels, and local and regional professional and business offices that diversify employment opportunities in the community.
- f) Strongly encourage the development of commercial and mixed-use projects that provide office space and associated uses that serve both the local neighbourhood and the wider region.
- g) Encourage local commercial development along arterial or collector streets in residential areas to provide additional amenities within walking distance.
- h) Encourage new daycare facilities as part of new development to help meet the needs of young families and provide local employment opportunities.



3.2 Residential

Another key guiding principle of the Plan is to **Provide Housing Options**. The Plan aims to provide a range of housing options in different forms, locations and densities, to offer choice for a variety of family types at all life stages; while respecting established areas where minimal change is anticipated.



Policies:

- a) Provide for a range of housing types, unit sizes, and tenures to appeal to a broad mix of households and different stages of life. This can include student housing, seniors housing, **family housing**, lock-off suites, and purpose-built rental, building on the policies in the *Housing Affordability Strategy (HAS)*.
- b) Focus higher density housing in the Burquitlam and Lougheed Neighbourhood centres and within 400 m of the Frequent Transit Network (FTN) as defined by TransLink.
- c) Encourage purpose-built rental and other **more affordable forms in transit-oriented locations and other areas**, utilizing the incentives in the *HAS* and explore partnerships with other levels of government for below market rental and housing ownership options.
- d) Ensure that any tenants displaced by redevelopment are provided with relocation assistance from the developer as required by the *HAS*.
- e) Promote the design of residential buildings, sites, and units to be adaptive and accessible for persons at different stages of life and degrees of mobility.
- f) Design all dwelling units at or near grade to face toward the street, lane, or walkway with direct access from an individual entrance. To ensure a degree of privacy, while maintaining natural surveillance of the street, design ground floor residential units to be slightly elevated above the sidewalk (except for designated accessible suites that require direct access to the street). Clearly delineate the separation between public and private space along streets and pedestrian corridors while allowing for natural surveillance of the public realm.
- g) Require multi-family and Housing Choices residential, commercial, and mixed-use development to manage solid waste by providing secure space and facilities for bear/wildlife-resistant garbage, organics and recycling collection as guided by the *Solid Waste Bylaw*.
- h) Retain the single-detached character of lands currently designated *One-Family Residential*, while continuing to provide opportunity for secondary suites and home-based businesses, in accordance with the City's existing policies and regulations.
- i) Council may give consideration to applications for RM-2 Three-Storey, Medium-Density Apartment Residential Zoning under the *Townhousing* land use designation where:
 - i. The proposal is for *Townhouse* use and does not contain *Apartment* uses;
 - ii. The site is not located next to an area designated *One-Family Residential*

3.3 Parks, Community and Civic Facilities

3.3.1 Parks, Recreation, and Culture

New and upgraded local parks and amenities are essential to ensure the growing Burquitlam-Lougheed neighbourhood is a great place to live. People living in urban areas, with smaller dwellings also have a greater need for parks and open space. A diverse park system can support the guiding principle of **Improve Community Amenities**. In concert with the urban design framework and greenways, these spaces help form the open space network.

Approximately 9.5 hectares (23.4 acres) of new parkland is proposed for the area over the next 20 to 25 years. All new parks and recreational amenities are guided by the *Parks, Recreation & Culture Master Plan* and associated park prioritization framework.

The open space network includes a series of publicly accessible spaces including parks, plazas, walkways, trails, streets, natural areas that together forms a linked network.

Table 2 – Existing and Proposed Parks

Park	Existing Area (ha)	Potential Increase (ha)	Potential Total Area (ha)
Oakdale Park	0.4	0.4	0.8
Miller Park	4.95	1.6	6.55
Cottonwood Park	0.9	3.87	4.76
Burquitlam Community Garden	0.4	0.2	0.6
Guilby Park	0.2	1.4	1.6
Lower Lougheed Park	0.2	0.4	0.6
Hart-Gauthier Park	-	0.8	0.8
Burquitlam Park ¹	1.55	- 0.9	0.65
Mountainview Park	1.78	-	1.78
Brookmere Park	2.17	-	2.17
Future parkland (Burquitlam)	-	1.6	1.6
Total:	12.6(30.8 ac)	9.37 (23.2 ac)	21.9 (54.1 ac)

¹ Reduction in Burquitlam Park size is due to the Community Recreation Facility being placed there. Cottonwood Park is being expanded to make up for this reduction.

Policies:

- a) Acquire additional parkland, over time, to meet community needs as identified in Table 2 and on Schedule A.
- b) Work towards the equitable provision of parkland that is accessible within a 10 minute walking distance for the majority of residents.
- c) Plan and design a system of parks, recreation and culture facilities, as guided by the *Parks, Recreation and Culture Master Plan*, that:
 - i. Aligns with the changing needs of a growing community and is inviting to newcomers;





Green infrastructure is the natural vegetation, soils, water and bioengineering solutions that collectively provide society with a broad array of products and services for healthy living. Natural areas such as forests, wetlands, and floodplains, and engineered systems like bioswales and rain gardens conserve natural resources and mitigate negative environmental effects, benefiting both people and wildlife. When green infrastructure is connected as part of a larger framework, a green infrastructure network is created.

-Connecting the Dots,
Metro Vancouver

- ii. Contributes to a sense of community and is accessible for people of all ages, abilities, incomes, and cultural backgrounds;
 - iii. Includes amenities and programming for people of different age groups and abilities;
 - iv. Encourages social interaction and active use throughout the day;
 - v. Allows for a variety of formal and informal uses, local events, and celebrations, where appropriate;
 - vi. Assesses parking demand and loading functionality at the time of new Citywide and community park development or park expansion.
- d) Integrate and promote public art as part of all new development in accordance with the *Public Art Policy*.
 - e) Encourage and foster public and private partnerships and other mechanisms that contribute to the expansion and provision of additional parks, recreation and culture amenities, services, and facilities.
 - f) Redevelop and transform Burquitlam Park to better complement the new and revitalized Neighbourhood Centre by shifting the program focus from active recreation to an urban community gathering place for social interaction, leisure activities, and city beautification.
 - g) Expand and revitalize Cottonwood Park to provide a more comprehensive program of amenities and facilities to better serve residents and visitors, including elements for families, youth, children, adults, seniors, formal and informal community gatherings, and active recreation.
 - h) All new development shall design any privately owned publicly-accessible outdoor spaces to contribute positively to the pedestrian experience and improve the overall public realm. These spaces shall connect, complement and enhance the publicly owned parkland, greenways, trails, and facilities in the neighbourhood.
 - i) Support the continued operation of the Vancouver Golf Club including expansion of the recreational services offered.

3.3.2 Schools, and Civic and Major Institutional

There are a number of schools and institutional facilities in Burquitlam-Lougheed that provide important public services, education, and amenities. Building on the guiding principle of **Improve Community Amenities** it will be important to ensure these facilities continue to meet the needs of the neighbourhood as it grows and evolves over time. Accordingly the Plan includes policies that encourage the expansion of these existing uses and facilities, as well as the development of new civic and major institutional uses, such as new community recreation centres and new seniors' housing that supports 'aging in place'.

Plan policies also aim to provide direct and safe walking and cycling connections to and from educational institutions, by connecting schools with neighbourhood centres, transit stations, parks, open spaces, and residential areas.

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Policies:

- a) Facilitate the development of a community recreation facility, in partnership with the YMCA of Greater Vancouver, at Burquitlam Park. The facility should provide a broad range of indoor and outdoor community recreation, activity, social service, and meeting spaces, that may be combined with other compatible uses (residential, commercial, institutional).
- b) Provide a new location for the Burquitlam Community Police Station that may be in conjunction with the development of the community recreation facility.
- c) Partner with School District 43 to expand, enhance and improve school facilities which benefit both the school and the community, and ensure the viability of school sites in the future.
- d) Work with School District 43 to ensure the retention of School District 43 land holdings to provide adequate space for future enrollment increases.
- e) Provide pedestrian and cyclist linkages that connect school sites with parks, open spaces and residential areas in the Neighbourhood to create greater opportunities for children to safely walk or cycle to/from schools and parks.
- f) Continue to provide internment and memorial services at the Robinson Memorial Park Cemetery.
- g) Develop public open space under and adjacent to the SkyTrain guideway along Clarke Road from Como Lake Avenue to Kemsley Avenue. These open spaces should provide a variety of uses, lighting, and amenities that contribute to a safe, positive, and active pedestrian experience and enhance neighbourhood beautification.
- h) Encourage new day care facilities as part of new development to help meet the needs of young families and effectively addresses the transition between new and existing adjacent development.





3.4 Heritage Conservation

To further the guiding principle of **Provide Housing Options** Burquitlam-Lougheed's heritage buildings and landscapes should be preserved and celebrated, where possible and feasible, as redevelopment occurs. Thus, Plan policies aim to assess older buildings, as guided by the City's Heritage Inventories, to determine if they are significant reminders of Burquitlam-Lougheed's social, cultural and architectural history.

The Plan encourages the use of Heritage Revitalization Agreements (HRAs) that preserve heritage buildings (or elements thereof) located on redevelopment sites, when supported through a heritage assessment, which is a market-driven approach that utilizes incentives and density bonuses to developers, in exchange for preservation and rehabilitation.

Policies:

- a) Prepare a heritage assessment for buildings listed on a Heritage Inventory at the time of redevelopment to assess the building's potential architectural, historical or contextual merit.
- b) Encourage the conservation of heritage buildings, as identified through a heritage assessment, through the use of Heritage Revitalization Agreements (HRA) as a part of redevelopment. Require Heritage Designation of any buildings that utilize heritage conservation incentives.

*Heritage buildings are those with either **Architectural** interest due to style, materials, structure, detailing, design, or architect; **Historical** interest due to events, trends, or people of interest associated with the building; or **Contextual** interest in conjunction with other historic resources or settings.*

3.5 Environment



The Burquitlam-Lougheed neighbourhood contains a number of important stream corridors and associated riparian areas that will continue to require protection as the neighbourhood grows, supported by the *Stoney, Chines, Austin and Rochester Creek Integrated Watershed Management Plans (IWMP)*.

Plan policies build on the guiding principle of **Foster Sustainability** and seek to protect and improve downstream water quality, fish and wildlife habitat and storm water management functions, through new development and infrastructure projects, as well as through partnership-based initiatives. Significantly, a section of the stream channel and adjacent riparian corridor of Stoney Creek extending from North Road to Chapman Avenue was protected under a federal *Species At Risk Act (SARA)* Critical Habitat Order for Nooksack Dace in May 2016.

Plan policies also work to reduce human-wildlife conflicts in concert with the *Solid Waste Management Bylaw* and *Wildlife and Vector Control Bylaw* by removing or minimizing wildlife attractants in landscaping, preventing wildlife access to solid waste, and following best management practices for urban wildlife in support of the City's *Bear Smart Certification*.

Policies:

- a) Require a Watercourse Development Permit for all development of land within a Riparian Assessment Area (RAA) as shown on Schedule G.
- b) For development of land adjacent to the Nooksack Dace Critical Habitat area, as shown on Schedule G, ensure that the features and attributes

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under the federal SARA Critical Habitat Order are protected, (i.e., protect the integrity of aquatic features and maintain a minimum riparian setback, as determined under the Riparian Areas Regulation (RAR) of the *Zoning Bylaw*).



- c) Implement recommended watercourse improvements as identified through Integrated Watershed Management Plans (IWMPs), including potential daylighting opportunities, improvements such as: bank stabilization at erosion sites, instream enhancements, riparian replanting, removal of invasive plant species, gravel recruitment, barrier removal and instream complexing, and channel linkage.
- d) Improve natural areas and wildlife corridors through new development, infrastructure projects, and other capital funding opportunities for areas in and around creeks and tributaries in the Stoney, Chines, Austin and Rochester Creek watersheds.
- e) Improve the riparian area, channel and banks of Stoney, Austin and Rochester Creeks to reduce the risk of flooding and improve water quality, fish habitat and stream health.
- f) Pursue through new development, infrastructure projects, partnerships and other funding sources, opportunities to rehabilitate natural areas for improved environmental function and quality.
- g) Require the use of native plant species in landscape plans for new development where appropriate to strengthen and restore riparian/wildlife habitat. Native plant material is more appropriately located in the interface with natural areas.
- h) Incorporate building designs that reduce energy use, increase energy efficiency, invest in renewable on-site energy and lower greenhouse gas emissions as guided by the *Community Greenhouse Gas Reduction Strategy*.
- i) Require the use of bear/wildlife-resistant garbage and recycling receptacles near and in parks, schools, urban trails, greenways and other areas of high pedestrian activity, as appropriate as guided by the *Solid Waste Management Bylaw* and in support of the *City's Bear Smart Certification*.
- j) Require multi-family and Housing Choices residential, commercial, and mixed-use development to manage solid waste by providing secure space and facilities for bear/wildlife-resistant garbage, organics and recycling collection as guided by the *Solid Waste Bylaw*.
- k) To preserve watershed health and allow for the free movement of wildlife, subject to engineering, geotechnical and environmental review and recommendations, and where warranted, the design of new or replacement crossings over watercourses shall consider:
 - i. Clear-span bridges instead of culverts in strategic locations; or
 - ii. Large culverts, with adequate space for bear/wildlife passage.



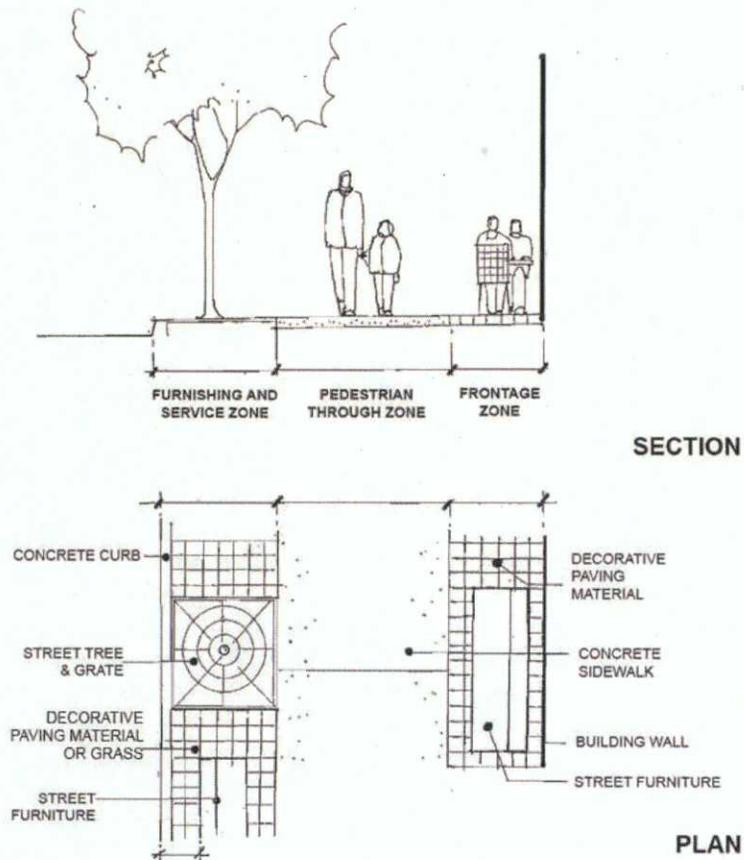
3.6 Transportation

In order for a neighbourhood to be successful, it is essential that people can conveniently access the places they live, work, shop, and play. One of the guiding principles of the plan is to **Increase Transportation Choice**. To foster this principle the City endeavours to make it easier to walk or cycle to SkyTrain stations, bus routes, and neighbourhood centres so more people can choose to leave their cars at home or even live without owning a car. A well-connected transportation network shortens travel distances, making it easier for people to quickly and conveniently walk or cycle to their destination, and shortens the journey by providing more direct routes.

Plan policies work in conjunction with the City's *Strategic Transportation Plan* (STP) to implement the guiding principle of increasing transportation choice, through strengthening transportation options in Burquitlam-Lougheed and developing a multi-modal street and path network that will improve the livability of the neighbourhood, contribute to the reduction of greenhouse gas emissions and enable healthier lifestyles. This approach will facilitate an increase in walking, cycling and transit trips.

"In lively, safe, sustainable and healthy cities, the prerequisite for city life is good walking opportunities ... a multitude of valuable social and recreational opportunities naturally emerge when you reinforce life on foot."
— Jan Gehl, *Cities for People*

Figure 5: Typical sidewalk cross-section and plan



3.6.1 Walking Policies:



- a) Provide publicly-accessible, mid-block walkways and statutory rights-of-way through, or adjacent to new development for all blocks longer than 125 metres, in order to improve pedestrian connectivity and accessibility.
- b) Establish a pedestrian-friendly public realm through streetscape improvements and redevelopment along designated greenways (as shown on Schedule C), commercial frontages (as shown on Schedule D), and throughout the Neighbourhood Centres (as shown on Schedule F). These streetscape improvements will include features such as wide sidewalks, corner bulges, mid-block walkways and crossings, street furniture, pedestrian-oriented lighting, and weather protection.
- c) Upgrade walkways to provide universal accessibility, improve the pedestrian experience, and enhance safety.
- d) Design any walkway over 75 metres in length to have a minimum 4.0 metre right-of-way, additionally walkways in high-density areas shall have a minimum 6.0 metre right-of-way.
- e) Implement universally accessible design features on sidewalks and at intersections to accommodate those with visual, mobility, and/or cognitive impairments.
- f) Locate all above-ground infrastructure, (e.g. utility kiosks, traffic-signal boxes, fire hydrants, street lights), in the furnishing and service zone between the curb and the sidewalk, ect. In no case shall this infrastructure intrude into the through-movement zone or create barriers for accessibility.
- g) Provide additional pedestrian crossings of arterial and collector streets and include, where feasible, measures that reduce crossing distance, and improve safety and visibility for pedestrians.
- h) Implement the *Pedestrian Wayfinding Plan* through additional signs and kiosks as new development occurs.

3.6.2 Cycling Policies:

- a) Improve intersection crossings for cyclists along identified bicycle routes, along Citywide Greenways and Neighbourhood Greenways in accordance with Schedule C.
- b) Provide public bicycle parking at strategic destinations in the plan area, including Neighbourhood Centres, schools, civic facilities, and parks.
- c) Work with the City of Burnaby and TransLink to improve pedestrian and cycling linkages to Lougheed Town Centre and the Central Valley Greenway.
- d) Work with the Ministry of Transportation and Infrastructure, the City of New Westminster and TransLink to improve pedestrian and cycling linkages across Highway 1, at the Brunette Interchange, to improve access to Braid SkyTrain Station.
- e) Work with the City of Port Moody and TransLink to improve pedestrian and cycling linkages between Coquitlam and Port Moody.
- f) Work with TransLink to explore secure bike parkades at SkyTrain stations.



- g) Implement the City's Cyclist Wayfinding Plan through additional signs and markings as new development occurs.

3.6.3 Greenway Policies:

- a) Develop a network of Citywide and Neighbourhood Greenways to connect pedestrians and cyclists with key destinations, neighbourhood centres, public parks, and amenities as shown on Schedule C.
- b) Require development located along Greenways, as identified on Schedule C, to provide an enhanced walking environment that includes elements such as:
 - i. Wider sidewalks as identified in the *Burquitlam-Lougheed Streetscape Guidelines*;
 - ii. Cycling facilities for all ages and abilities;
 - iii. Where the Greenway is located outside of a street right-of-way it shall have a minimum right-of-way of 6.0 metres;
 - iv. Row of street trees on both sides of the sidewalk/pathway;
 - v. Seating and other appropriate street furniture;
 - vi. Public art;
 - vii. Pedestrian oriented lighting;
 - viii. Wayfinding; and
 - ix. Buildings shall address the greenway with active frontages, architecturally distinctive façades, and high-quality streetscape treatments.



Translink's Frequent Transit Network (FTN) is a network of corridors where transit service runs at least every 15 minutes in both directions throughout the day and into the evening, every day of the week.

3.6.4 Transit Policies:

- a) Continue to work with TransLink to review opportunities to improve both coverage and frequency along *Frequent Transit Network (FTN)* corridors, and achieve optimal transit service coverage in order to meet user demand as population and employment densities increase over time. This includes new routes, increased frequencies on existing routes and more direct services to connect the neighbourhood to rapid transit stations.
- b) Enhance transit stops with pedestrian amenities and weather protection through improved lighting, benches, shelters or awnings as part of the frontage improvements required by new development.
- c) Facilitate the transition of the Burquitlam Station bus exchange as shown in the *Burquitlam Station Area Transportation and Transit Integration Concept Plan* in conjunction with the redevelopment of the adjacent property.



3.6.5 Street Policies:

- a) Require development to provide new streets, lanes and street extensions, as shown on Schedule B, to support high-density growth and accommodate

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multi-modal permeability, circulation, and connections. The design of new streets and lanes will be guided by the *Strategic Transportation Plan (STP)*, *Subdivision and Development Servicing Bylaw*, *Burquitlam-Lougheed Streetscape Guidelines*, the *Burquitlam-Lougheed Servicing Assessment* and other relevant City policies, guidelines, and Bylaws.



- b) Upgrade streetscapes based on the *Burquitlam-Lougheed Streetscape Guidelines*.
- c) Upgrade intersections and incorporate appropriate measures to improve safety, reduce vehicle speeds, improve intersection safety, shorten pedestrian crossing distances and reduce pedestrian-vehicle conflicts.
- d) Require the creation of rear lanes, as part of redevelopment, for interior or “land-locked” parcels, particularly along arterials that do not have secondary access (e.g., North Road, Clarke Road, Como Lake Avenue, and Lougheed Highway).
- e) Work towards the provision for rear lanes to have two access connections to adjacent streets, and avoid creating dead-end lanes, where feasible.
- f) Establish a connected multi-modal transportation grid that supports increased connectivity and walkability with short block lengths, mid-block walkways, and new streets, lanes and pathways to target a maximum block length of 125 metres.
- g) Facilitate the transition of Lougheed Highway streetscape to an urban boulevard to improve walking and cycling facilities, including wider sidewalks, double rows of street trees, landscaped median, improved crosswalks, and new development fronting the street with active frontages as guided by the *Burquitlam-Lougheed Streetscape Guidelines*.
- h) Seek the provision of additional public right-of-way on all arterial streets, in accordance with the *Zoning Bylaw* and *Subdivision and Development Servicing Bylaw*, as part of redevelopment for sidewalk widening, cycling facilities, transit passenger facilities, street widening, and on-street parking provision, as guided by the *Burquitlam-Lougheed Streetscape Guidelines*.
- i) Seek the provision of additional public right-of-way on North Road from Delestre to Austin Avenue to provide for a third northbound travel lane.
- j) Goods movement policy for North Road and Lougheed Highway is to be developed as per the City of Coquitlam *Strategic Transportation Plan*.

3.6.6 Parking Policies:

- a) Encourage shared, common off-street non-residential parking to serve multiple users and destinations in the Neighbourhood Centre, as well as civic and institutional uses.
- b) Implement the *Public Parking Strategy in Rapid Transit Station Areas* to effectively manage on-street parking resources.
- c) Encourage parking turnover and efficient use of on-street and off-street parking in the Neighbourhood Centres, including areas in and around SkyTrain stations, through the implementation of a variety of parking management strategies (e.g., time limits and pricing).

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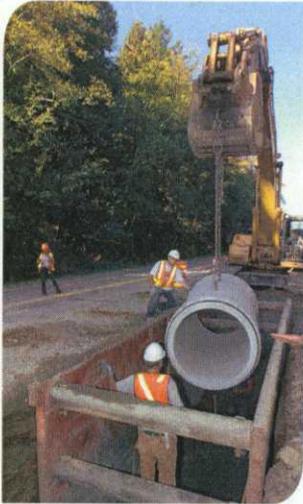
- d) Provide reductions in the number of required parking spaces as guided by the Transportation Demand Management (TDM) measures in the *Zoning Bylaw*.
- e) Provide additional public parking as part of the development of the community recreation facility in conjunction with the YMCA of Greater Vancouver.
- f) Provide signage to direct motorists to off-street parking facilities in order to ensure their optimal use and reduce unnecessary circling.
- g) Locate all off-street parking for commercial, high-density residential and medium-density residential development underground or concealed within a building.
- h) Facilitate sufficient on-site loading space to minimize impacts to on-street parking.

3.6.7 Transportation Demand Management:

- a) Require new development to provide Transportation Demand Management (TDM) measures in order to obtain reductions in minimum parking requirements in accordance with the *Zoning Bylaw*.
- b) Require short-term and long-term bicycle parking for new development in accordance with the *Zoning Bylaw*.
- c) Facilitate an expansion in the number of publicly available car-sharing vehicles.
- d) Ensure there is well-designed and clear wayfinding signage to serve people who take transit, cycle and walk.
- e) Seek the provision of end-of-trip facilities for cyclists in all major developments.
- f) Encourage carpooling to reduce peak period road use and make more efficient use of road space.

3.7 Utility Servicing

The provision of adequate utility servicing (water, sewer, stormwater) is necessary to facilitate new residential, commercial and mixed-use growth in Burquitlam-Lougheed, as guided by the Plan's vision and principles. New utility infrastructure in public space will be well integrated into the urban landscape to avoid obstructing pedestrian movement. The rainwater management practices, as outlined in Integrated Watershed Management Plans (IWMP), will improve the ecology and hydrology of watercourses. Plans for water supply and distribution, sanitary sewer, and stormwater servicing requirements to implement the Neighbourhood Plan are included in the *Burquitlam-Lougheed Servicing Assessment*.



Policies:

- a) Ensure that stormwater management facilities conform with the respective IWMPs for Stoney Creek, the Chines, and Austin Creek and Rochester Creek.
- b) Apply the City's Rainwater Management Guidelines to all subdivision and building permit applications in the Neighbourhood Plan area, including those not covered by an adopted IWMP.
- a) Plan and coordinate the location of utility boxes (e.g. telephone hubs and electrical transformers), ventilation equipment or other at-grade mechanical equipment to minimize their impact on the public realm, including placing them underground or inside buildings wherever possible.
- b) Place overhead utilities underground as part of new development and infrastructure upgrade projects in all commercial, high-density and medium-density residential areas.
- c) Improve the riparian area, channel and banks of Stoney, Austin and Rochester Creeks to improve stormwater management and reduce the risk of flooding.



4.0 URBAN DESIGN POLICIES

Urban design involves the design of buildings and the spaces between them to create livable, attractive and human-scaled buildings, streets, neighbourhoods, public spaces and parks.

Good design encompasses all of the guiding principles and Plan policies. These aim to improve neighbourhood character and the pedestrian experience along the main streets of the area, to meet the principle of *Design on a Human Scale*. This involves applying the policies below, the complementary Development Permit Guidelines and Burquitlam-Lougheed Streetscape Guidelines.

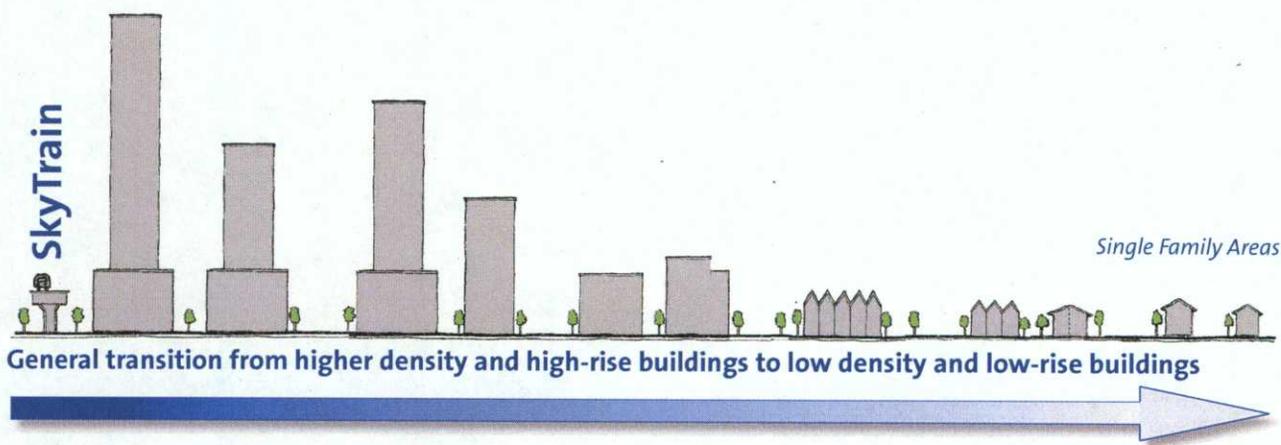
In Burquitlam-Lougheed, urban design principles play an important role in ensuring new development is walkable and transit supportive, has a high-degree of architectural excellence, complements the public realm, fits appropriately into existing neighbourhoods and enhances neighbourhood character.

Also, based on the importance of the new SkyTrain stations and the topographical and view-rich context, these policies also strongly encourage a high degree of architectural excellence, especially for buildings and developments located at key intersections, gateways and viewpoints.

Human-scaled design refers to urban design that promotes people friendly places, building designs, and urban patterns focusing on a pedestrian-oriented public realm based on human dimensions and experiences.

4.1 General Policies

- a) Require all commercial, mixed-use, multi-family residential, and Housing Choices (with two or more principal dwelling units on a lot) developments to obtain a Development Permit to provide for higher-quality project design.
- b) A density bonus above the maximum base gross floor area, as specified in the *Zoning Bylaw*, may be offered in exchange for the provision of amenities or a specified financial share of the land value of the additional density to be applied towards the provision of amenities deemed by the City to have public benefit. Examples of these amenities include: community facilities, park improvements, plazas, streetscape improvements, transit infrastructure, daycares, walkways, greenways, affordable housing, and public art. Density bonus contributions will be allocated towards specific amenities as determined by Council.





- c) Development densities and building heights will be highest in and adjacent to the Burquitlam and Lougheed Neighbourhood Centres, along major arterial streets and adjacent to TransLink's *Frequent Transit Network* (FTN). Buildings will generally transition to lower height with increased distance from the Neighbourhood Centres, in consideration of local context. The manner and style of transition will not be the same in all areas.
- d) Land assemblies and consolidations must follow a logical pattern to achieve future transportation connections as shown on Schedule B to leave no remnant (orphan) parcels that are undevelopable under their designated land use.
- e) Integrate soft landscaping, surface texture and other enhancements to the design of retaining walls where they are located along any public right-of-way, in a public space or are visible to the public.
- f) Encourage the use of Green Streets that convert portions of local streets into public open green spaces in the neighbourhood to provide additional space for gathering, leisure and relief from the built environment. Green streets may have a mix of hard and soft landscaping and adjoining buildings shall actively front Green Streets. The location and design of Green Streets will be guided by the following:
 - i. Comprehensive development occurring along the subject street frontage to minimize disruption to local residents;
 - ii. A transportation network analysis demonstrating that the street can be closed to vehicles;
 - iii. Sufficient rear lane access;
 - iv. Sufficient space for parking and loading;
 - v. Sufficient fire access; and
 - vi. Site Appropriate Landscaping.
- g) New developments shall locate buildings and site towers in a manner that respects the opportunities for future redevelopment of adjacent parcels.

4.2 Street Frontage Type Policies

- a) Provide continuous ground-oriented, at-grade, pedestrian-focused, retail, commercial, or civic/assembly uses in the ground floor of all buildings along frontages identified on Schedule D as *Mandatory Commercial Street Frontage*.
- b) All development along *Mandatory Commercial Street Frontage* should include the following:
 - i. Additional sidewalk width, as determined in the *Burquitlam-Lougheed Streetscape Guidelines*, with a wider frontage zone to provide opportunities for outdoor seating and display.
 - ii. All entrances that are slightly recessed at the grade of the adjoining sidewalk should prevent door swing from impeding the through-movement zone of the sidewalk.





- iii. Integrate pedestrian-oriented streetscapes and an enhanced public realm to establish direct and seamless connections that direct pedestrians to the Burquitlam and Lougheed Neighbourhood Centres.
- iv. Orient buildings and closely integrate building design to address and create a strong connection with the street.
 - v. Require continuous weather protection (canopies, awnings) for pedestrians on building facades with no gaps wherever possible as well as outdoor seats adjacent to buildings.
 - vi. Incorporate pedestrian-oriented lighting.
 - vii. Encourage options for pedestrian seating along and adjacent to buildings, separate from buildings or sidewalks.
- c) Provide continuous, ground-oriented, at-grade, commercial, civic/assembly, employment living, institutional, or residential uses on the ground floor of all buildings along frontages identified on Schedule D as *Optional Commercial Street Frontage*.
- d) Provide continuous ground-oriented (or slightly elevated) residential, civic/assembly, employment living, institutional, or highly-transparent residential amenity uses on the ground floor of all buildings along frontages identified on Schedule D as *Residential Street Frontage*.
- e) Discourage on-site loading and solid waste collection along *Mandatory Commercial Street Frontage*.

4.3 Urban Design Framework - Building Design Policies

The Urban Design Framework elements discussed below are not limited to locations identified on Schedule E. Through the development process additional locations for these elements may be identified.

- a) Require development that is identified as:
 - **Landmark Sites**, as identified on Schedule E; and/or
 - **Landmark Buildings**, that have a proposed height of 30 storeys or greater;

to provide a high degree of design excellence due to their prominence and location within the neighbourhood. **Landmark Sites** and **Landmark Buildings** will be designed through a rigorous design-review process that includes:

- i. Following the 'Landmark Buildings' Development Permit Guidelines located in Part 4 of the CWOCP;
- ii. Elements that provide a high degree of design excellence such as public art, signage, unique public space, plazas, distinctive lighting, unique and high-quality building materials, sculpted architectural form, unique building floorplates, distinctive façades, site furnishing, improved street presence, and streetscape treatments that signal entry to distinct areas and/or identification of precincts.
- iii. Design input by City staff at key intervals;
- iv. Outside professional design review;
- v. Requirement for significant public realm improvements and/or public space contribution as detailed in 4.4

Design Excellence:

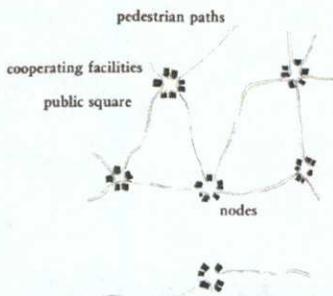
An exemplary building for its time and place, which has perceived lasting value that shows a connection to its place and offers a sense of delight.





- vi. Professional studies to ensure all potential community impacts (e.g., transportation, shadow, wind, environmental) are considered and addressed in the building and site design;
 - vii. Robust community consultation conducted by the applicant that is in addition to any statutory consultation process required for their development application. This will include an early presentation of the concept to Council at the pre-application stage. The consultation process will receive public input to ensure all potential community impacts from the landmark building and site are considered, including the proposed design of the building and site. The results of the consultation process, including how the proponent addressed community impacts, will be submitted to the City for consideration by Council.
- b) Require development located at **Gateways**, as identified on Schedule E, to include elements that provide a high degree of design excellence and mark entry to the neighbourhood, such as:
- public art,
 - signage,
 - unique public space and plazas that convey a sense of entry,
 - distinctive lighting,
 - high-quality building materials,
 - sculpted architectural form,
 - distinctive façades,
 - high-quality site furnishing,
 - improved street presence, and
 - streetscape treatments that signal entry to distinct areas and/or identification of precincts.

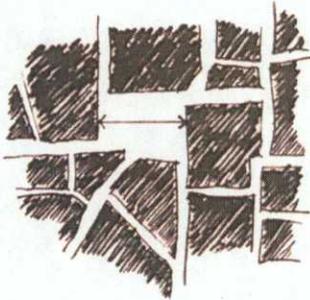
4.4 Urban Design Framework - Public Space Policies



*"Studies of pedestrian behaviour make it clear that people seek out concentrations of other people, whenever they are available."
- A Pattern Language: Towns, Buildings, Construction by C. Alexander, et al.*

- Local public space, through additional building setbacks, that is distributed throughout the neighbourhood is key to supporting the open space network. The Urban Design Framework on Schedule E contains a series of Primary, Major and Minor Nodes to enhance the public realm.
- a) Further to the Node locations identified on Schedule E, Nodes should be particularly located on the northeast corner of intersections, and at public walkway connections that intersect with streets, to take advantage of the southwest aspect.
 - b) All Nodes will be designed to encourage public gathering and shall include elements such as:
 - i. A hard-surface area of at least 50%;
 - ii. Seating and other appropriate street furniture;
 - iii. Public art, water features and other elements that promote pedestrian interest;

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"A town needs public squares; they are the largest, most public rooms, that the town has."

- C. Alexander,
A Pattern Language



"Nodes are natural centres of activity in the urban environment."

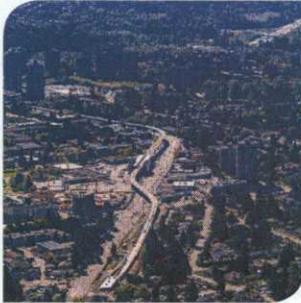
- C. Alexander,
A Pattern Language

- iv. Use of trees and other vertical elements to define pedestrian-scale spaces;
 - v. Fronting buildings shall address the node with active frontages, architecturally distinctive façades, and high-quality streetscape treatments.
- c) Further to the requirements in 4.4 b) development located at **Primary Nodes**, as identified on Schedule E, shall provide a publicly-accessible open space which is a minimum of 250 square metres (with one dimension between 12 metres to 35 metres). Primary nodes will be designed to encourage a broad range of public gathering activities and shall include elements such as:
- i. A portion with weather protection coverage to facilitate year-round use;
 - ii. A mix of hard and soft landscaping that does not impede safety nor sightlines to commercial uses and building entrances;
 - iii. Pedestrian oriented lighting;
 - iv. Three-phase electrical and non-potable water connections;
 - v. Wayfinding;
- d) Further to the requirements in 4.4 b) development located at **Major Nodes**, as identified on Schedule E, shall provide a publicly-accessible open space that is approximately 45 square metres. Major nodes will be designed to encourage public gathering and shall include elements such as:
- i. Pedestrian oriented lighting; and
 - ii. Wayfinding.
 - iii. Fronting buildings shall have direct building frontage on at-least one side.
- e) Further to the requirements in 4.4 b) development located at **Minor Nodes**, as identified on Schedule E, shall provide a publicly-accessible open space that is approximately 16 square metres.

5.0 DISTRICT POLICIES

Based on the Plan vision, principles and land use concept, this section contains several detailed policies for specific districts within the Plan area as shown on Schedule F.

5.1 Burquitlam Neighbourhood Centre



The Burquitlam Neighbourhood Centre is the heart of the Burquitlam Neighbourhood, focused on Clarke Road. Building on its designation as a *Frequent Transit Development Area* (FTDA) in the City's *Regional Context Statement* (RCS) the neighbourhood centre is evolving into a walkable, high-density, mixed-use area with a high degree of design excellence. The Neighbourhood Centre is an area where there are local shops and services that provide the goods of daily life, where there are public spaces and places for people to meet and engage in community life, and where people can move easily and without a car to access places to work, play, and shop.

Policies:



- a) All streets in the Neighbourhood Centre shall provide continuous building frontage with active uses along the ground floor in accordance with Schedule D.
- b) A continuous streetwall (building façade) or podium of a minimum of three storeys shall be provided along North Road and Clarke Road. High-rise buildings should be stepped back from the street façade, as appropriate. Well-integrated breaks are appropriate for nodes, small plazas and ground level lobby entrances to residential uses located on upper storeys, provided they are carefully designed to respect the continuous storefront character and retail viability of the street.
- c) Accommodate the development of community facilities, such as childcare services, that respond to residential and employment growth and contribute to neighbourhood livability.
- d) Encourage the development of a variety of commercial spaces including local and regional serving office space.
- e) Encourage the development of connected urban squares, plazas, courtyards, walkways, and other outdoor open spaces, throughout the Neighbourhood Centre and adjacent to Clarke Road and North Road to contribute to the pedestrian focus of the shopping streets:
 - i. Require commercial uses along ground floor building areas that front onto squares/plazas, with allowance for entrances leading to other uses in buildings.
 - ii. Design interfaces between streets, squares/plazas, walkways, and buildings to include seating areas, patios and landscaping that support 'active' commercial uses (e.g. restaurants, cafes and outdoor display).
- f) Create a high quality pedestrian realm that encourages walking along Como Lake Avenue and Clarke Road through the provision of wide sidewalks, landscaping, special paving treatments, coordinated street furniture, and optimized pedestrian travel through the intersections. These



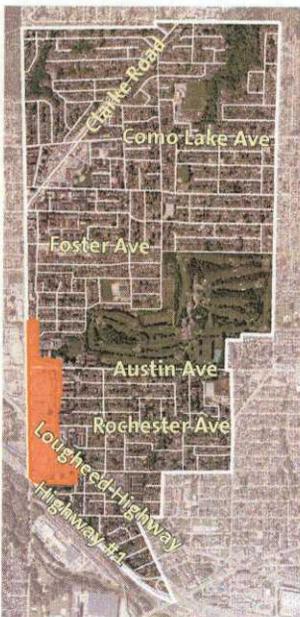


improvements will be guided by the *Burquitlam-Lougheed Streetscape Guidelines*.

- g) Through the redevelopment of the property surrounding Burquitlam SkyTrain station, ensure the station area is well served with access for bus movement (as shown in the *Burquitlam Station Area Transit Integration Concept Plan*), in a manner that is fully integrated with the urban design vision of the Burquitlam Neighbourhood Centre for a highly walkable and integrated station area.

5.2 Lougheed Neighbourhood Centre

Active ground floors are those that contain many small units that are highly transparent and have a large variety in function and façade relief. Articulation is primary vertical with good details and materials.



The Lougheed Neighbourhood Centre is an important commercial corridor along North Road that interfaces with the Lougheed Town Centre across the street in Burnaby. Designated as a *Municipal Town Centre* in the *City's Regional Context Statement (RCS)* the Neighbourhood Centre is evolving into a walkable, high-density, mixed-use area with a high-degree of design excellence. The Neighbourhood centre will continue to provide a range of local and regional serving retail and office opportunities complemented by high-rise residential.

Policies:

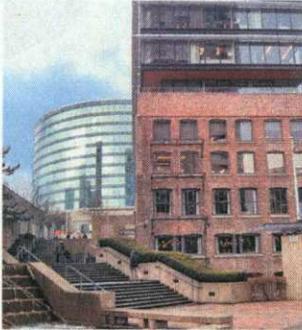
- a) All streets in the Neighbourhood Centre shall provide continuous building frontage with active uses along the ground floor in accordance with Schedule D.
- b) A continuous streetwall (building façade) or podium of a minimum of three storeys shall be provided along North Road. High-rise buildings should be stepped back from the street façade, as appropriate.
- c) Accommodate the development of community facilities, such as childcare services that respond to residential and employment growth and contribute to neighbourhood livability.
- d) Encourage the development of a variety of commercial spaces including local and regional serving office space.
- e) Encourage the development of connected urban squares, plazas, courtyards, walkways, and other outdoor open spaces throughout the Neighbourhood Centre and adjacent to North Road to contribute to the pedestrian focus of the shopping street:
 - i. Require commercial uses along ground floor building areas that front onto squares/plazas, allowing entrances to lead to other uses in the building.
 - ii. Design interfaces between streets, squares/plazas, walkways, and buildings to include seating areas, patios and landscaping that support active commercial uses (e.g., restaurants, cafes and outdoor display).
- f) Create a high-quality pedestrian realm that encourages walking along North Road and Lougheed Highway through the provision of wider sidewalks, landscaping, special paving treatments, coordinated street furniture, and optimized pedestrian travel through the intersection.

9.2

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These improvements will be guided by the *Burquitlam-Lougheed Streetscape Guidelines*.

- g) Integrate high-quality connections for pedestrians and cyclists to negotiate up and down the escarpment. These should be linked to an improved walkway system and the greenway network.
- h) Develop new streets as part of new development to break up the commercial superblocks and provide improved local street access from North Road.
- i) Acquire additional street right-of-way dedication to provide for three northbound travel lanes on North Road in addition to any dedication required to provide wider sidewalks and landscaped boulevard, as guided by the *Subdivision and Development Servicing Bylaw* and the *Burquitlam-Lougheed Streetscape Guidelines*.
- j) Relocate the existing signalized intersection at North Road/Delestre Avenue to North Road and the new east-west street through the Neighbourhood Centre as shown on Schedule B.



5.3 North Road

North Road is a key 'Main' street in Coquitlam that connects the Burquitlam and Lougheed Neighbourhood Centres and interfaces with the Lougheed Town Centre in Burnaby on the west side of the street. As the area grows, North Road will transform into a walking, cycling, and transit-friendly urban boulevard. As an urban boulevard, North Road will have active uses that address the street, street trees to frame the sidewalk and the street.

Policies:

- a) Foster strong multi-modal transportation connections to provide easy access to the Burquitlam and Lougheed Neighbourhood Centres.
- b) New development will directly address and frame the street with active ground-floor frontages and a minimum streetwall height of three-storeys.
- c) As with the portion of North Road south of Austin Avenue, acquire additional street right-of-way dedication along this section to provide for three northbound travel lanes in addition to any dedication required to provide wider sidewalks and landscaped boulevards as guided by the *Subdivision and Development Servicing Bylaw* and the *Burquitlam-Lougheed Streetscape Guidelines*.
- d) Introduce new, controlled intersections and crosswalks to provide additional, safe, crossing opportunities for walking and cycling.
- e) Designate the blocks east of Whiting Way from Ebert Avenue south to Brookmere Avenue as the Whiting Way/Appian Way Future Planning Area (as identified on Schedule F). Recognizing that this area is a key transition zone from the North Road corridor and is in close proximity to both Burquitlam and Lougheed stations. [Completed as per SWCAP Policy 9.1 and Schedule E](#), this study will take into consideration the





higher density land uses, housing types, form of development, land use transition, amenities, transportation improvements and utility servicing that is appropriate for this area.

- f) Coordinate with the City of Burnaby on this plans for the development of Lougheed Town Centre and the interface across North Road.

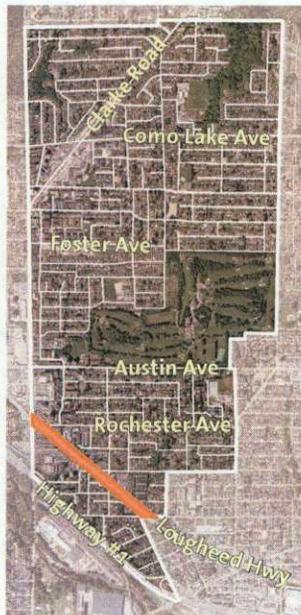
5.4 Oakdale

Oakdale consists of primarily single-family dwellings that blend into the Glenayre area in Port Moody to the north. The lower third of Oakdale is within a 5 to 7 minute walk (400 metres to 600 metres) of the high-density Burquitlam Station area. In the area beyond, a 7 to 10 minute walk from the station, Oakdale exhibits a change to a larger single-family lot and longer block pattern, complete with well known 'nature trail' walkways, the Stoney Creek natural area, an adjacent school site and Burnaby Mountain Park to the west.



Policies:

- a) Retain and enhance the existing 'nature trail' walkways, Northern Red Oak Footpath, Pin Oak Footpath, Garry Oak Footpath, and White Oak Footpath as landscaped walking and cycling routes and connect these walkways into the greenway network.
- b) Provide a new plaza/public space under the Skytrain guideway at Clarke Road and Como Lake Avenue, and seek to provide a pedestrian connection to Elmwood Street and/or Gardena Drive as part of future planning.
- c) Designate the Oakdale Neighbourhood as a Future Planning Area recognizing this area's proximity to Burquitlam Station and the Burquitlam Neighbourhood Centre. Completed following SWCAP Policy 9.1 and Schedule E, this future plan will take into consideration the higher density land uses, housing types, form of development, land use transition, amenities, transportation improvements and utility servicing that is appropriate for the Oakdale area (Schedule F).
- d) Acquire additional parkland over time, to expand Oakdale Park and provide a new 0.4 to 0.8 hectare park in a location to be determined through in the future Oakdale Local Area Plan. The design of these parks will be guided by the Parks, Recreation and Culture Master Plan.



5.5 Lougheed Boulevard

Lougheed Highway is a key arterial street in Coquitlam and part of TransLink's Major Road Network (MRN) connecting the City to the wider region. As the Lougheed Neighbourhood grows, the Lougheed Highway public realm will transform into a walking, cycling, and transit friendly boulevard. The urban boulevard will have new buildings fronting the street, street trees to frame the street, bike lanes and wide sidewalks.

Policies:

- a) New development will directly address and frame the street with active ground-floor frontages and a minimum streetwall height of three-storeys.
- b) Facilitate an improved public realm through additional setbacks or street dedication to provide wider sidewalks and landscaped boulevards.
- c) Introduce new controlled intersections and crosswalks to provide additional, safe, crossing opportunities for walking and cycling.
- d) Monitor and adjust speed limits along Lougheed Highway as required to suit the level of redevelopment within the district.

5.6 Coquitlam College

The Coquitlam College area, **should the property owner initiate redevelopment by applying to the City**, is envisioned to transform into a higher-density, mixed-use precinct that integrates education facilities, residential, and supportive commercial opportunities in an open campus setting.

Policies:

- a) **Require the applicant to prepare** a Comprehensive Master Plan, as outlined in Policy the CWOCP which accommodates the redevelopment of the Coquitlam College.
- b) Encourage a variety of housing types including student-focused housing and/or “lock-off suites”.
- c) Locate taller buildings at the west end of the site, adjacent to Whiting Way, with a decrease in height as you move east, to a maximum of four stories adjacent to Brookmere Park to minimize shadow impacts on the Park.
- d) Locate a maximum three-storey streetwall with residential or institutional uses for portions of the site directly fronting onto Brookmere Avenue.
- e) Encourage the development of connected squares, plazas, courtyards, parks and other outdoor open spaces throughout the site to contribute to the open campus character; connect with Brookmere Park and the surrounding neighbourhood. These open space(s) should total a minimum of 0.2 hectares in size and complement Brookmere Park. These open spaces shall be designed to promote public gathering and sitting opportunities, plus an area with weather protection coverage to facilitate year-round use.
- f) Improve and enhance Brookmere Park with added park amenities and improved walking and cycling connections; including improved sightlines from adjacent streets. These connections should be linked to an improved walkway system and the greenway network.
- g) Improve Austin Creek and its tributaries through redevelopment, including daylighting opportunities, following the recommendations in the *Austin / Rochester Integrated Watershed Management Plan (IWMP)*.
- h) Accommodate the development of community amenities, such as educational facilities, childcare services, and local commercial that contribute to neighbourhood livability.





6.0 DEVELOPMENT PERMIT AREAS

6.1 Development Permit Areas

Innovative design solutions, that achieve high quality urban environments that are walkable and liveable are encouraged in the Plan area.

- a) In addition to the specific guidelines in this Plan, Development Permit Guidelines that apply within the Burquitlam-Lougheed Neighbourhood Plan area are located in Part 4, Chapter 13.0 of the CWOC.



7.0 IMPLEMENTATION AND MONITORING

7.1 Implementation

The Plan will guide growth and reinvestment in the Burquitlam-Lougheed Neighbourhood to help create complete communities over the next 20 to 25 years. Realization of this long-term vision will occur incrementally, as development proceeds, as amenity and infrastructure investments are made to encourage and accommodate growth in Burquitlam-Lougheed. It will also require the application of coordinated policies, inter-departmental plans and periodic updates to City bylaws and regulations.

The following tasks will help implement the Plan and facilitate future growth and change in the Neighbourhood.

- a) Prepare amendments to the *Zoning Bylaw* to:
 - i. All townhouse, apartment and mixed-use zones to:
 - Encourage purpose-built rental housing, as guided by the *Housing Affordability Strategy* (HAS), including a review of density, parking, amenity space and other requirements.
 - Accommodate secondary or “lock-off” suites (self-contained dwelling units within apartment or townhouse units) as a permitted use.
 - Encourage a wider range of housing options to appeal to a broad mix of household types, including 3-bedroom units.
 - ii. RT-2 Townhouse Residential zone to increase the allowable height to 3-storeys in transit-oriented development areas.
 - iii. RT-1 Two-Family Residential and RT-3 Triplex and Quadruplex Residential zones to promote sensitive in-fill that helps to refresh neighbourhoods and fosters expanded housing options to meet the needs of a diverse range of household types.
 - iv. C-5 Community Commercial zone to provide density and height guidance for locations outside of the Austin Heights and Maillardville Neighbourhood Centres.
 - v. All commercial zones to promote economic development, facilitate local employment opportunities and encourage office development. Explore amendments to the C-7 Transit Village Commercial Zone to increase the minimum employment generating floorspace requirement to increase local employment opportunities and promote economic development.
- b) Develop an inter-departmental Servicing Assessment that describes the phasing of improvements to utilities, transportation and parks facilities relative to development as it occurs. It should also outline the timing and departmental responsibility to effectively implement the Plan.
- c) Develop Streetscape Standards to direct the development of frontage improvements to the streets and public rights-of-way and the provision of street furniture.

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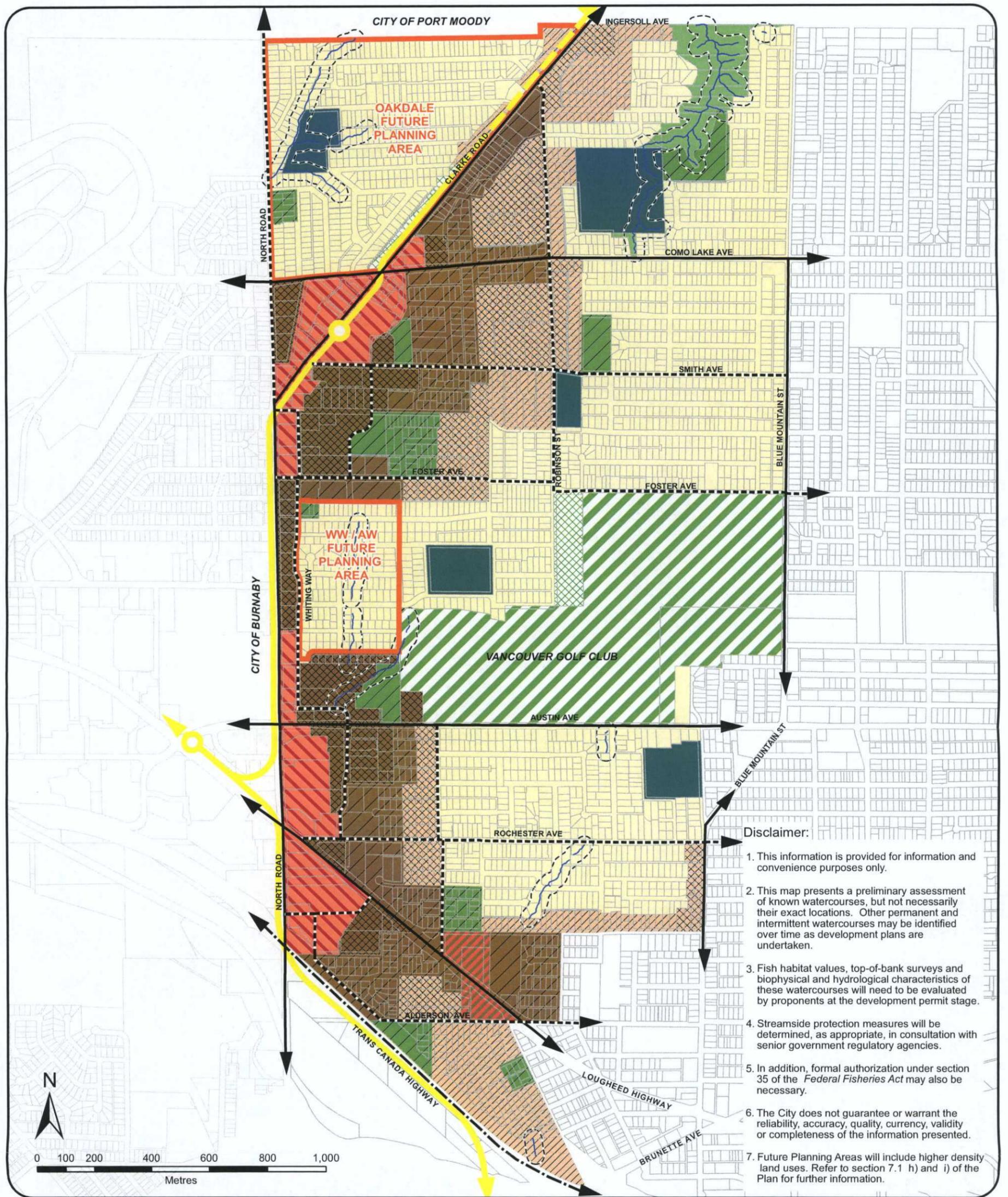
- d) As part of a broader work program item, prepare a *Heritage Management Plan* that includes Heritage Revitalization Agreement (HRA) guidelines and procedures that provide direction for the conservation of heritage buildings and use of these agreements. These guidelines will provide criteria for determining appropriate density, form of development, siting, character, access, parking and loading functionality, and incentives.
- e) Explore developing a Heritage Character Area for the 500 and 600 block of Rochester Avenue in conjunction with the preparation of the *Heritage Management Plan*.
- f) Prepare amendments to the *Zoning Bylaw, Urban Design + Development Permit Areas, Solid Waste Management Bylaw* and/or *Wildlife and Vector Control Bylaw* to:
 - i. Require the use of native plant species in landscape plans for new developments, and for the improvement and restoration of riparian and wildlife habitat, and to apply the use of native plant material in the interface with natural areas.
 - ii. Require multi-family and Housing Choices residential, commercial, and mixed-use development to manage solid waste by providing secure space and facilities for bear/wildlife-resistant garbage, organics and recycling collection as guided by the *Solid Waste Bylaw*.
- g) Develop a Transportation Demand Management (TDM) policy with a suite of potential TDM measures that may be utilized to justify parking relaxations as guided by the *Zoning Bylaw*.
- h) In keeping with the SWCAP Neighbourhood Policy Framework, Policy 9.1 and Schedule E, at a future date prepare a local area plan for the Oakdale Neighbourhood that recognizes the area's proximity to Burquitlam Station and the Burquitlam Neighbourhood Centre. This plan will take into consideration higher density housing types, form of development, land use transition, parks, amenities, transportation improvements and utility servicing that is appropriate for the Oakdale area.
- i) Conduct a land use policy review for the Whiting Way/Appian Way Future Planning Area as identified on Schedule F. This study will take into consideration land uses, and transitions of the housing types, form of development, density, parks, amenities, transportation improvements and utility servicing that is appropriate for this area.
- j) The above-mentioned future planning processes (7.1h and 7.1i) may be initiated where:
 - i. Council directs staff to prioritize the work item on the City's Business Plan and undertake a planning process and;
 - ii. A significant number of residents petition Council to initiate planning work and council directs staff to undertake a planning process; or
 - iii. A major development application is submitted to the City that proposes an OCP amendment in one of these areas and Council directs staff to conduct a broader planning review.
- k) Prepare a daycare policy to foster the development of new daycares that includes guidelines on location, design and open space.

- l) Prepare a Corridor Development Strategy for major arterial streets in Southwest Coquitlam that takes into consideration land use, housing types, form of development, land use transition, urban design, public space, amenities, transportation improvements and utility servicing that is appropriate for these corridors.

7.2 Monitoring

In order to ensure that the Burquitlam-Lougheed neighbourhood is developed in a manner consistent with the vision, principles and policies of this Plan, City staff will monitor and report back to Council on an annual basis:

- a) Development of housing, by number and type of units;
- b) Development of commercial floor space, by amount and type;
- c) Provision of public amenities;
- d) Transportation mode share, the amount of sidewalk and cycling facilities constructed, transit service coverage and parking provisions;
- e) Implementation and effectiveness of the stormwater management system in relation to the *Austin/Rochester Creeks and Stoney Creek IWMPs*;
- f) Implementation, effectiveness and appropriateness of provincial and municipal best management practices for urban wildlife;
- g) Implementation of strategies to improve natural areas and wildlife corridors through new development infrastructure projects;
- h) Improvements and frequency of new development that adopts the use of native plant species; and
- i) Incorporate designs that will lower greenhouse gas emissions.



- Disclaimer:**
1. This information is provided for information and convenience purposes only.
 2. This map presents a preliminary assessment of known watercourses, but not necessarily their exact locations. Other permanent and intermittent watercourses may be identified over time as development plans are undertaken.
 3. Fish habitat values, top-of-bank surveys and biophysical and hydrological characteristics of these watercourses will need to be evaluated by proponents at the development permit stage.
 4. Streamside protection measures will be determined, as appropriate, in consultation with senior government regulatory agencies.
 5. In addition, formal authorization under section 35 of the *Federal Fisheries Act* may also be necessary.
 6. The City does not guarantee or warrant the reliability, accuracy, quality, currency, validity or completeness of the information presented.
 7. Future Planning Areas will include higher density land uses. Refer to section 7.1 h) and i) of the Plan for further information.

SCHEDULE A - BURQUITLAM - LOUGHEED NEIGHBOURHOOD PLAN

LAND USE DESIGNATIONS

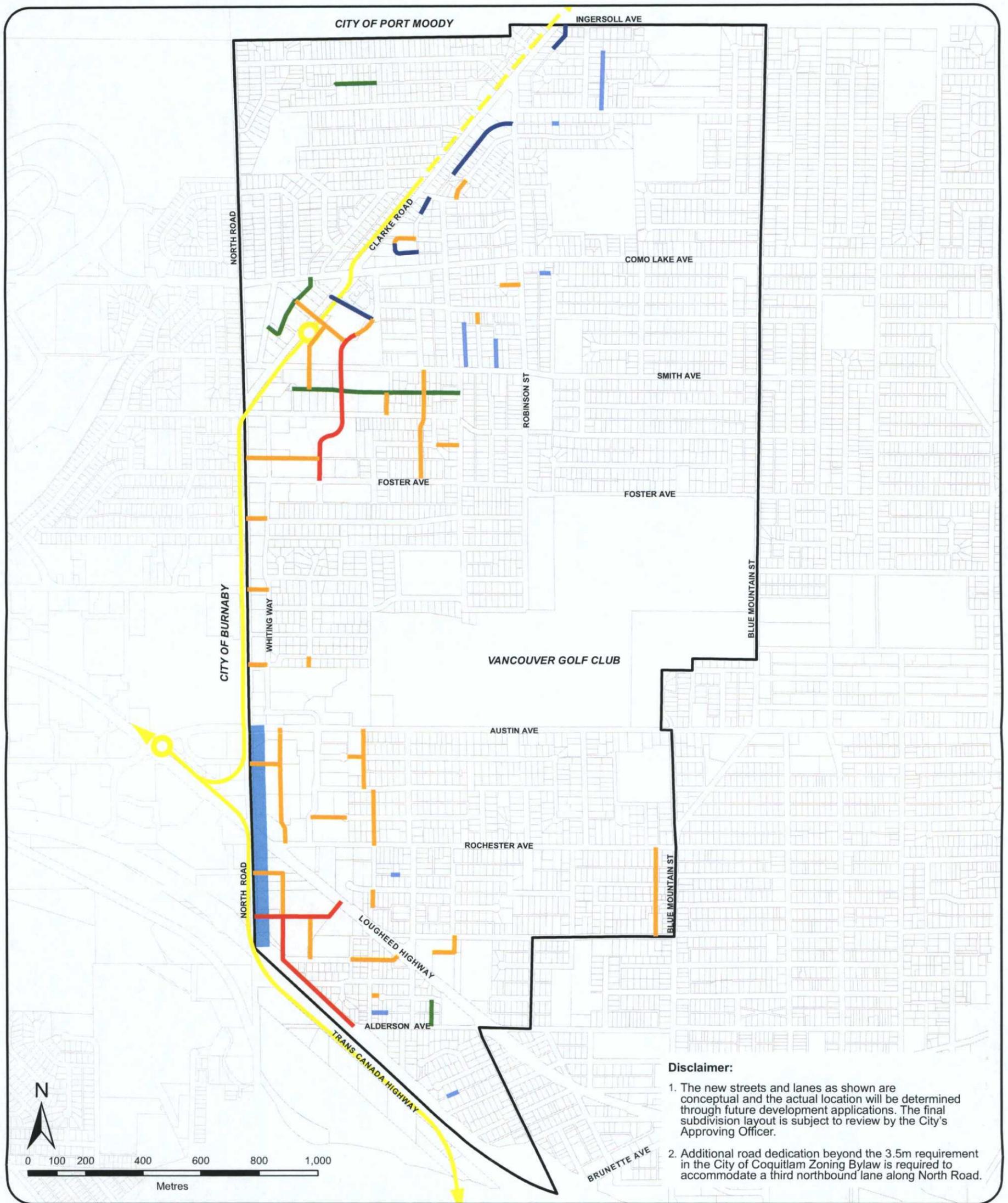
Transit Village Commercial	One Family Residential	Municipal Arterial Street
Neighbourhood Centre	Civic and Major Institutional	Municipal Collector Street
High Density Apartment Residential	School	Provincial Highway
Medium Density Apartment Residential	Parks and Recreation	SkyTrain
Low Density Apartment Residential	Natural Areas	SkyTrain Station
Townhousing	Extensive Recreation	Watercourse
Neighbourhood Attached Residential	Future Planning Area ⁶	Riparian Assessment Area (RAA)

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Adopted: XXX XX, XXXX
Amended: N/A

Prepared By: Planning & Development
Source: City of Coquitlam - OCP GIS layer

Coquitlam



Disclaimer:

1. The new streets and lanes as shown are conceptual and the actual location will be determined through future development applications. The final subdivision layout is subject to review by the City's Approving Officer.
2. Additional road dedication beyond the 3.5m requirement in the City of Coquitlam Zoning Bylaw is required to accommodate a third northbound lane along North Road.

**SCHEDULE B - BURQUITLAM - LOUGHEED NEIGHBOURHOOD PLAN
NEW STREETS AND LANES**

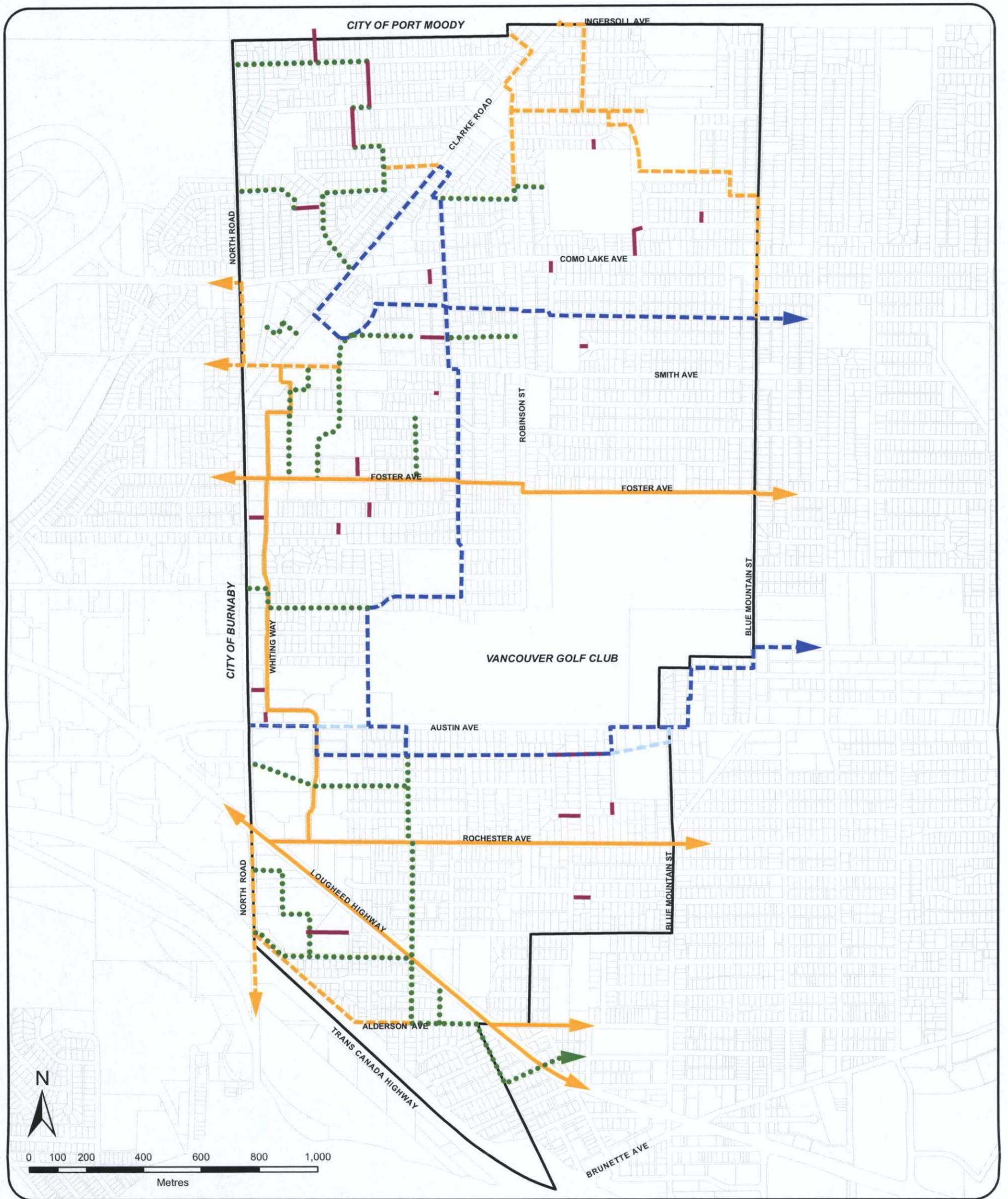
- Municipal Collector
- High Density Local Road
- Narrow Street
- Primary Access Lane
- Standard Lane
- Additional Dedication ²
- SkyTrain
- SkyTrain Station
- Burquitlam - Lougheed Neighbourhood Plan Boundary

DRAFT

Adopted: XXX XX, XXXX
Amended: N/A

Prepared By: Planning & Development
Source: City of Coquitlam - OCP GIS layer





SCHEDULE C - BURQUITLAM - LOUGHEED NEIGHBOURHOOD PLAN

PROPOSED GREENWAYS AND CYCLE ROUTES

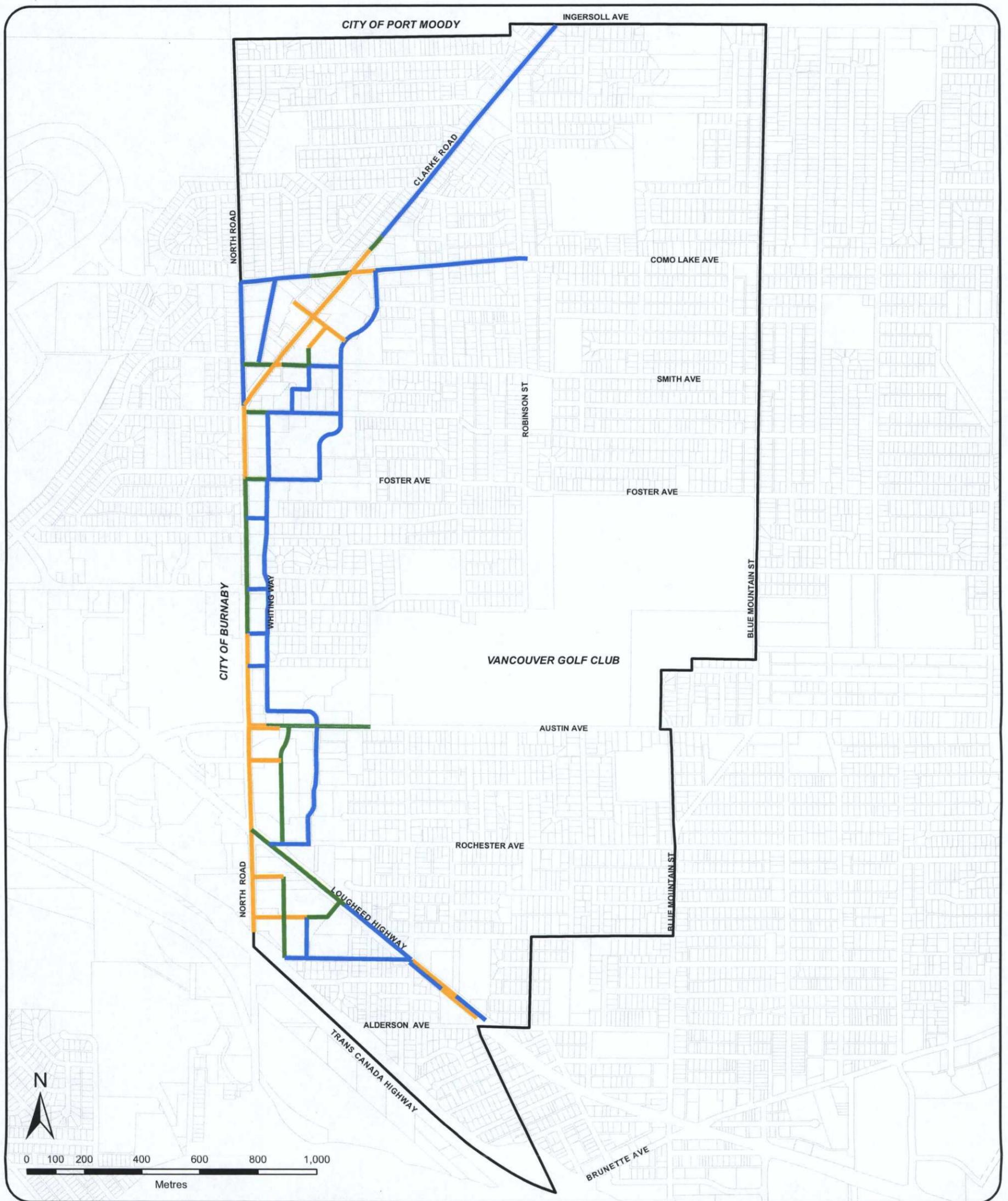
- Proposed Citywide Greenway
- Proposed Citywide Greenway Alternate
- Proposed Neighbourhood Greenway
- Existing Cycle Route
- Proposed Cycle Route
- Existing Walkway
- Burquitlam - Lougheed Neighbourhood Plan Boundary

DRAFT

Adopted: XXX XX, XXXX
Amended: N/A

Prepared By: Planning & Development
Source: City of Coquitlam - OCP GIS layer

Coquitlam



SCHEDULE D - BURQUITLAM - LOUGHEED NEIGHBOURHOOD PLAN
STREET FRONTAGE HIERARCHY

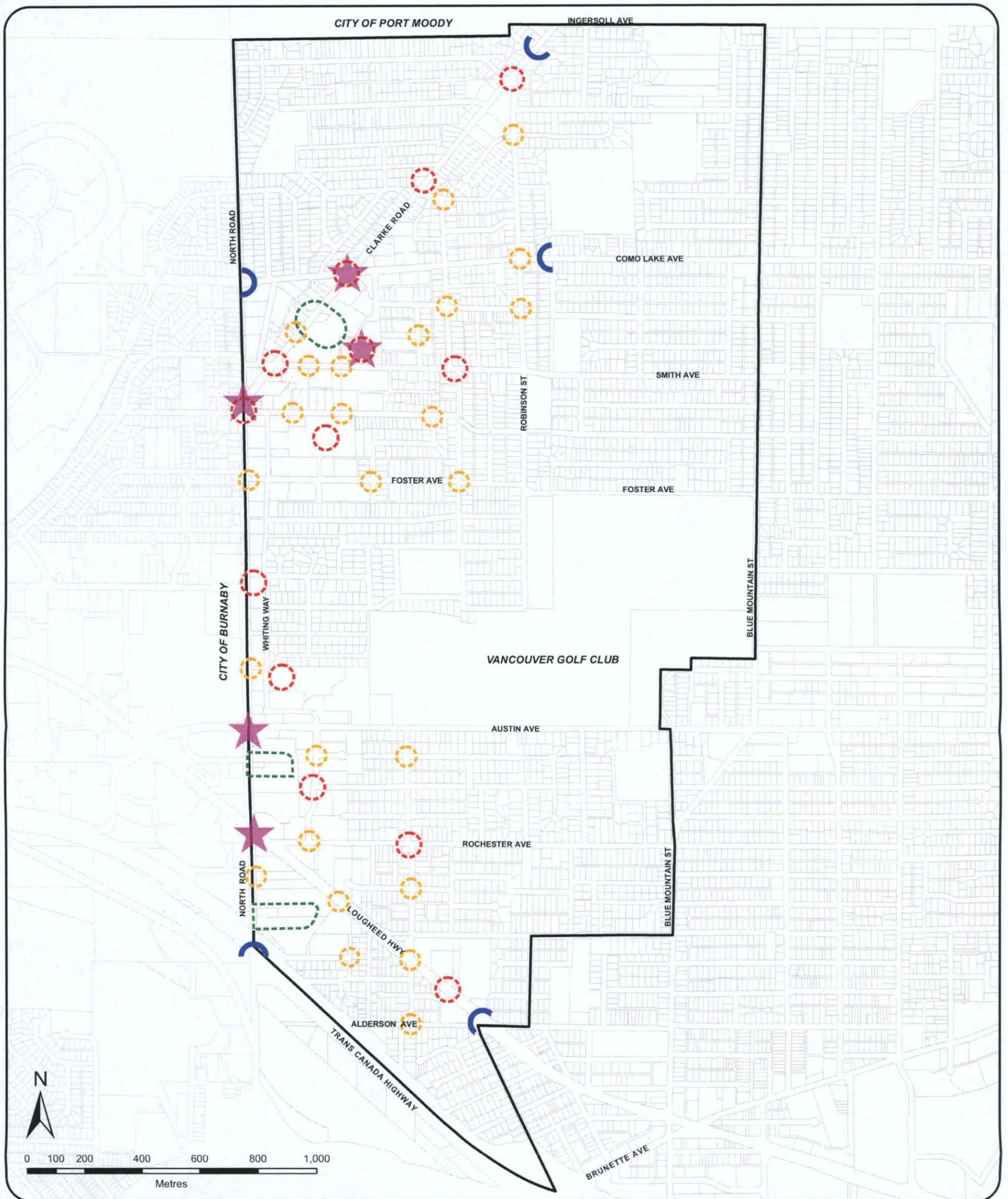
- Mandatory Commercial Street Frontage
- Optional Commercial Street Frontage
- Residential Street Frontage
- Burquitlam - Lougheed Neighbourhood Plan Boundary

DRAFT

Adopted: XXX XX, XXXX
 Amended: N/A

Prepared By: Planning & Development
 Source: City of Coquitlam - OCP GIS layer





SCHEDULE E - BURQUITLAM - LOUGHEED NEIGHBOURHOOD PLAN
URBAN DESIGN FRAMEWORK

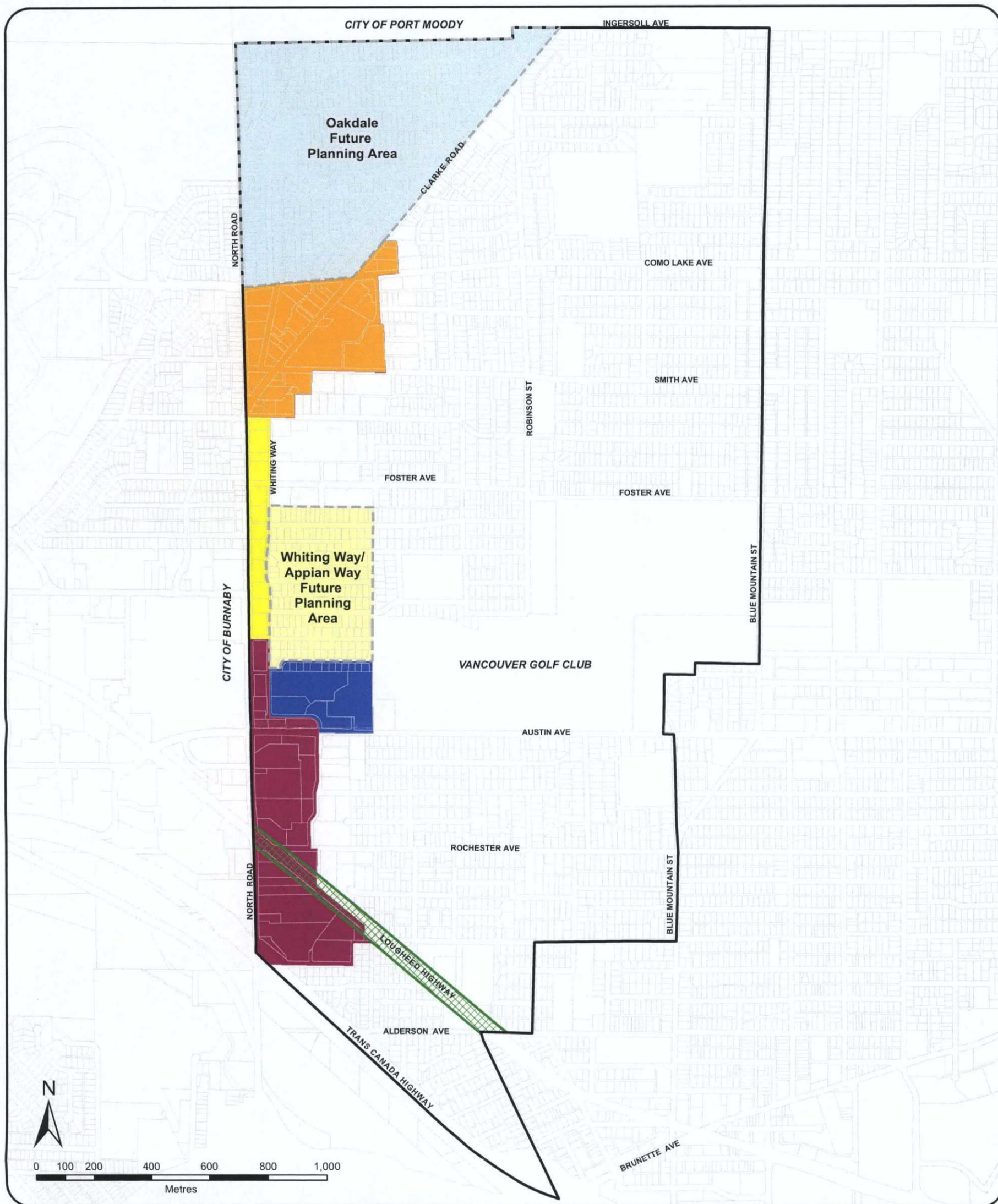
-  Primary Node
-  Major Node
-  Minor Node
-  Burquitlam - Lougheed Neighbourhood Plan Boundary
-  Landmark
-  Gateway

DRAFT

Adopted: XXX XX, XXXX
 Amended: N/A

Prepared By: Planning & Development
 Source: City of Coquitlam - OCP GIS layer





SCHEDULE F - BURQUITLAM - LOUGHEED NEIGHBOURHOOD PLAN

DISTRICTS

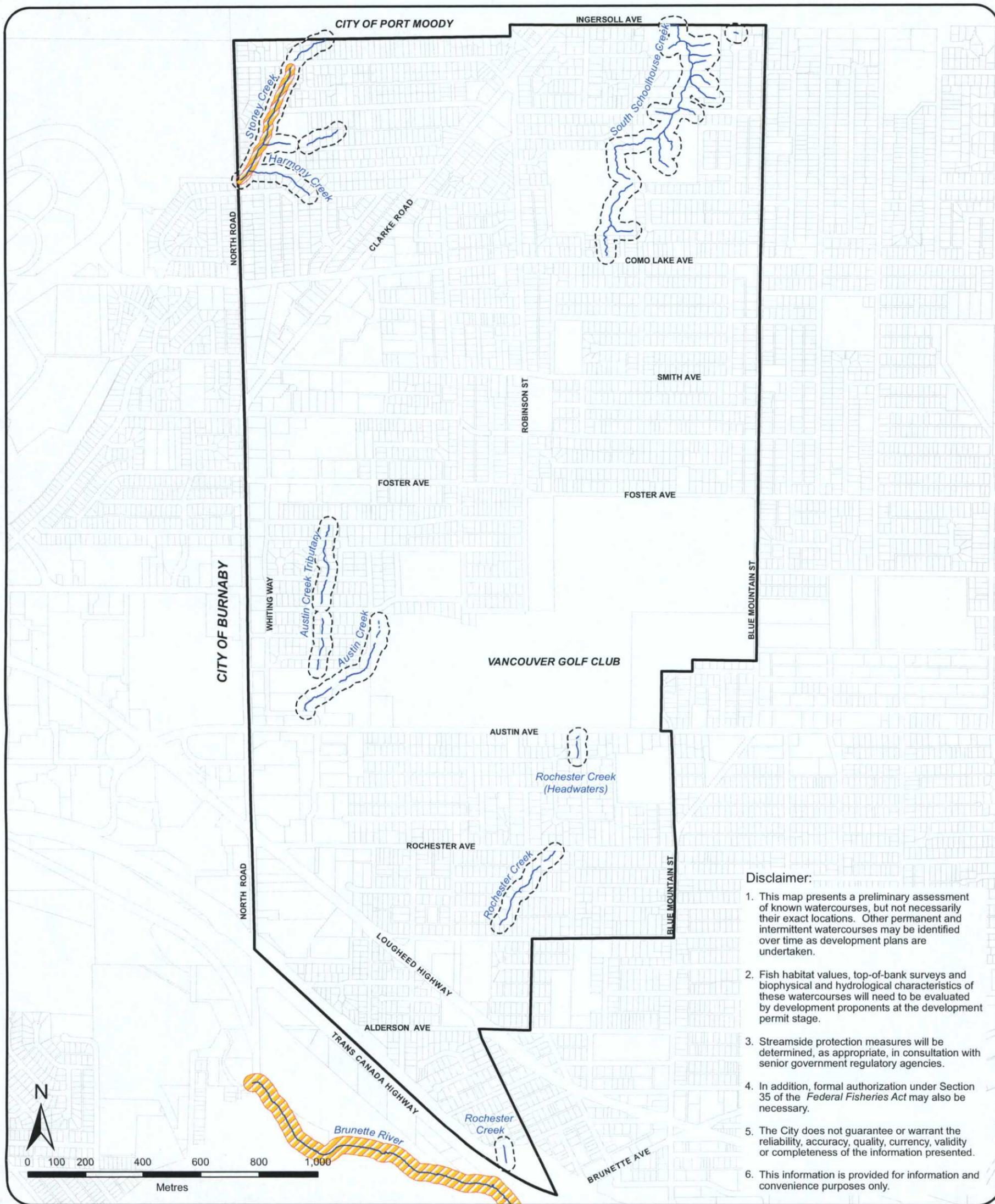
- Oakdale Future Planning Area
- Burquitlam Neighbourhood Centre
- North Road
- Whiting/Appian Way Future Planning Area
- Lougheed Neighbourhood Centre
- Coquitlam College
- Lougheed Boulevard
- Burquitlam - Lougheed Neighbourhood Plan Boundary

DRAFT

Adopted: XXX XX, XXXX
Amended: N/A

Prepared By: Planning & Development
Source: City of Coquitlam - OCP GIS layer

Coquitlam



- Disclaimer:**
1. This map presents a preliminary assessment of known watercourses, but not necessarily their exact locations. Other permanent and intermittent watercourses may be identified over time as development plans are undertaken.
 2. Fish habitat values, top-of-bank surveys and biophysical and hydrological characteristics of these watercourses will need to be evaluated by development proponents at the development permit stage.
 3. Streamside protection measures will be determined, as appropriate, in consultation with senior government regulatory agencies.
 4. In addition, formal authorization under Section 35 of the *Federal Fisheries Act* may also be necessary.
 5. The City does not guarantee or warrant the reliability, accuracy, quality, currency, validity or completeness of the information presented.
 6. This information is provided for information and convenience purposes only.

**SCHEDULE G - BURQUITLAM - LOUGHEED NEIGHBOURHOOD PLAN
WATERCOURSES AND RIPARIAN AREAS**

LEGEND

-  Watercourse
-  Riparian Assessment Area (RAA)
-  Nooksack Dace Critical Habitat protected under Federal Species at Risk Act
-  Burquitlam - Lougheed Neighbourhood Plan Boundary

DRAFT

Adopted: XXX XX, XXXX
Amended: N/A

Prepared By: Planning & Development
Source: City of Coquitlam - OCP GIS layer



BURQUITLAM-LOUGHEED NEIGHBOURHOOD PLAN – PROPOSED CWOCF AMENDMENT**Part 2, Chapter 2, Section 2.1.1****Comprehensive Development (CD) Zones**

- a) The use of Comprehensive Development (CD) zones may be considered in any Land Use Designation based on the following criteria:
- i. Large site over 2.5 hectares that includes multiple buildings and/or uses;
 - ii. Small sites with significant constraints;
 - iii. Multiple phases;
 - iv. Multiple uses, tenures and ownerships (i.e. multiple stratas and/or airspace parcels);
 - v. Significant public amenity and/or community benefit;
 - vi. Significant housing affordability component; and/or,
 - vii. Complicated or unique servicing, public amenity, and design schemes that require variances, incentives and innovative approaches.

Part 2, Chapter 2, Section 2.1.2**Comprehensive Master Plan Policies:**

- a) All development sites greater than 2.5 hectares and/or proposing greater than 50,000 square metres gross floor area shall prepare a comprehensive master plan, which is secured through a Development Agreement, concurrent with rezoning. The plan shall include, but not be limited to, the following elements to the satisfaction of the City:
- i. A site plan showing the general location and uses of all buildings, streets and public spaces;
 - ii. Density and building height ranges for each development parcel taking into consideration factors such as view line potential, shadow impacts and wind effects, and traffic and pedestrian circulation;
 - iii. Development program and site statistics, including development areas, land use mix, residential and commercial unit types;
 - iv. The appropriate zones that will be applied;
 - v. Greenways and/or green streets as appropriate;
 - vi. A public space, amenities and benefits strategy that incorporates parks, publicly-accessible open spaces and/or plazas that comprise at least 5% of the total site area and total a minimum of 0.2 hectares in size;
 - vii. Landscaping plan (including interim landscaping between phases);
 - viii. Transportation strategy for vehicles, cycling and walking, with an emphasis on supporting a pedestrian-friendly environment;
 - ix. Parking and loading strategy;
 - x. Urban Design Framework plan to detail key corridors, precincts, nodes, landmarks, gateways, streetscapes, and other public realm features and outline their design treatment;
 - xi. Public art plan;
 - xii. Sustainability Strategy;
 - xiii. Utilities servicing concept, including a storm water management plan;
 - xiv. Phasing plan;
 - xv. Securities, as appropriate, to secure infrastructure and public benefit.

Part 2, Chapter 2, Section 2.3.1

SkyTrain Guideway Integration

The SkyTrain guideway is a significant piece of transportation infrastructure, and adjacent development requires an appropriate design response. The elevated guideway also provides opportunities to enhance street-level public space in the neighbourhood.

Policies:

- a) Development within 10 metres of the SkyTrain guideway or within 25 metres of a station shall follow TransLink's Adjacent and Integrated Development (AID) project consent process.
- b) Develop public space under the guideway in a way that integrates with the North Road and Clarke Road enhancements, animating this space and improving natural surveillance. This space should contribute to a positive pedestrian experience with appropriate lighting and clear sightlines to the street.
- c) Encourage development under or around the guideway subject to TransLink consent.
- d) New commercial uses fronting the guideway should be designed to create active edges that are highly integrated with areas under the guideway and visible from the adjacent street. This should be a seamless and complementary connection.
- e) Development fronting the SkyTrain guideway shall incorporate measures to mitigate noise impacts and allow the housing units to meet Canada Mortgage and Housing Corporation's (CMHC) guidelines for maximum acceptable levels of noise in dwellings. These measures may include:
 - i. **Sensitive site and building design:** placing noise sensitive rooms away from noise sources; locating hallways, stairwells and utility areas closer to noise sources; and using single-loaded building design;
 - ii. **Improving building construction:** triple-glazed windows; additional wall insulation; locating vents and ducts away from noise sources; alternative ventilation systems; sound dampening or absorptive walls and cladding materials; and concrete construction; and

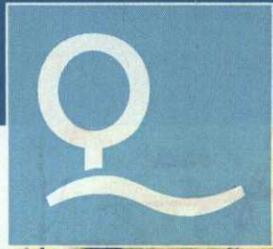
Noise buffers: solid glazed balconies; fences; sound absorptive landscaping; and water features.

Part 2, Chapter 8, Section 8.2.1

Plan Amendment Criteria

The OCP is intended to provide a flexible framework to guide development over the next 20 to 25 years. Recognizing that areas evolve and change over time, proposed plan amendments may be considered, provided they address the following evaluation criteria:

- a) Provide a comprehensive planning rationale;
- b) Provide a property size / assembly rationale;
- c) Further implement the vision, principles, and policies of the OCP;
- d) Further implement the policies of another City of Coquitlam plan or strategy;
- e) Secure additional community benefit;
- f) Secure a housing affordability component;
- g) Facilitate an improved land use transition between building forms;
- h) Consider the impact on transportation and infrastructure;
- i) Consider community consultation outcomes;
- j) Lack of capacity for the proposed form of development in the local area;
- k) Where the amendment is to facilitate increased density, the proposed site shall meet at least one of the following location criteria:
 - i. Be located directly adjacent to an existing area of the proposed designation;
 - ii. Be located within a designated Municipal Town Centre or Frequent Transit Development Area as defined in the Regional Context Statement
 - iii. Be within 400 m of the Frequent Transit Network (FTN);
 - iv. Be located on an arterial or collector street.



City of Coquitlam

Citywide Official Community Plan, Part 4

Urban Design + Development Permit Areas

*Managing & Shaping
the Built Environment*

AMENDED XXXX X, 2017 - BYLAW XXXX, 2017
AMENDED APRIL 1, 2014 - BYLAW 4465, 2014
AMENDED JULY 8, 2013 - BYLAW 4391, 2013
ADOPTION MARCH 18, 2013 - BYLAW 4295, 2013

Coquitlam
Planning &
Development

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Introduction



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1.1 Urban Design

The City welcomes innovative design for high-quality urban environments that are walkable, prosperous, safe and environmentally sustainable.

Urban design refers to the design of buildings and the relationships between buildings as well as the spaces between them (the public realm), with the aim of creating livable, attractive and human-scaled streets and neighbourhoods.

In growing communities, urban design plays an important role in ensuring new development is walkable, transit supportive, fits appropriately into existing neighbourhoods, and enhances neighbourhood character.

Public Realm refers to all parts of the city which are publicly accessible. It includes all streets, walkways, squares, parks and open spaces as well as semi-public spaces.

1.1.1 CITYWIDE DESIGN VISION

Coquitlam is envisioned as a series of compact, complete communities in concert with nature. All new development should reinforce this vision.

A street level focus is important to help create a lively, people-centered environment. Wide sidewalks, street trees, and shops opening onto the street will be designed to help foster a better environment for businesses and the public. Abundant connections allow for easy pedestrian movement.

Buildings will be designed to provide a sense of street enclosure and create a degree of comfort that is common to great urban experiences around the globe, while minimizing environmental impacts. Our vibrant urban villages will form the centres of public life while residential streets will be livable for people at all stages of their lives. To foster a people-focused environment, parking will be designed to integrate and respect all other aspects of the public realm.

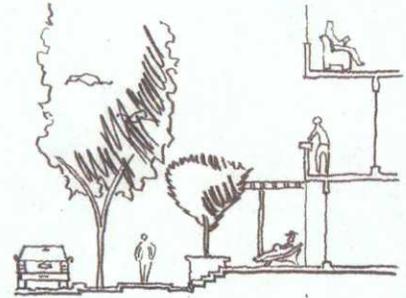
Public plazas accommodate outdoor cafés, restaurants, festivals, and public markets. High-quality landscaping and street furniture will improve our quality of life.

1.1.2 CITYWIDE DESIGN PRINCIPLES

Urban design influences our quality-of-life. Coquitlam’s urban design principles are key to making places that thrive both socially and economically, are desirable to live in and attractive to visit. A well designed building is one with a fine-grain that directly fronts onto a street and has active uses at ground level where the building meets the sidewalk.

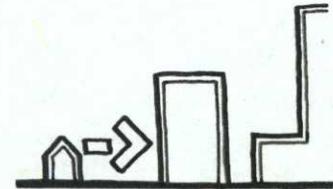
a. People-friendly Buildings & Streets

People-friendly buildings and streets are designed to be safe, comfortable and attractive to walkers. Buildings will front onto streets with active ground floors. Buildings and blocks will be permeable to allow people to easily move around and through the City and parking will be hidden. In public places, the design should provide ample sunlight, gathering spaces and seating opportunities.



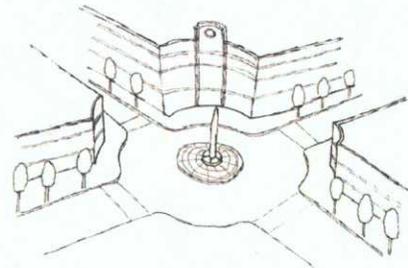
b. Good Neighbour

All development should be designed in a manner that is neighbourly and is in harmony with the scale and character of its surroundings while minimizing adverse impacts on adjacent properties. Development often occurs incrementally and therefore the design must carefully consider future relationships with surrounding properties and the public realm.



c. Place Making

Each neighbourhood will have a unique sense of place and identity based on its physical setting, landmarks, and cultural history. Architectural excellence and distinction are encouraged. Buildings that fit appropriately into their surroundings, are aesthetically attractive and are functional for their intended use demonstrate design excellence. Public art should be employed to enhance local areas, and act as gateways into specific neighborhoods.



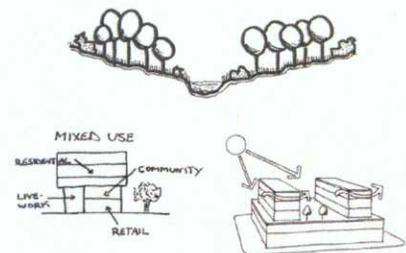
d. Safety and Accessibility

Quality building design and site planning can play a role in reducing opportunities for crime and the perception or fear of crime. All areas of the City shall be accessible for all types of people at all stages of their life.



e. Sustainable Development

All development should demonstrate a comprehensive approach to sustainability that is reflected in the design and operation of the building. This includes the need to protect, restore and manage ecosystems, to conserve energy and resources, and reduce waste. New developments should create opportunities to ensure long-term prosperity while helping to satisfy the needs of the present and future generations.



1.2 Development Permit Areas

All Development Permit Areas have a consolidated set of guidelines in common in order to achieve the City's design objectives.

1.2.1 DEVELOPMENT PERMIT GUIDELINE STRUCTURE

The Development Permit Guidelines are divided into several sections:

- **2.0 Citywide Guidelines** that apply to all types of development in all Development Permit Areas, unless otherwise noted.
- **3.0 Building Type Guidelines** that apply in addition to the Citywide Guidelines and address certain types of issues specific to different building types in all development permit areas.
- **4.0 Neighbourhood Specific Guidelines** that apply to all development within a specific Development Permit Area and supplement the Citywide and Building Type guidelines.
- **5.0 Environmental Guidelines** that apply for the protection of the natural environment or the protection of development from hazardous conditions.

Should a conflict exist between a citywide guideline or a building type guideline and a neighbourhood specific guideline, the neighbourhood specific guideline shall take precedent.



1.2.2 STATUTORY CONTEXT

Subject to the provisions of the *Local Government Act*, as amended from time to time, new development, as described and defined in the City's *Zoning Bylaw*, within the Development Permit Areas identified in Section 1.2.3 will be subject to issuance of a Development Permit.

These Development Permit Areas and associated guidelines prescribe the general criteria for new development and form the basis for the review and approval of a development permit. The City may require modification to a Development Permit application for failure to meet the standards of these guidelines. All Development Permit Areas (DPA's) are designed to control development for one or more of the following purposes:

- Protection of the Natural Environment , its ecosystems and biological diversity;
- Protection of development from hazardous conditions;
- Protection of farming;
- Revitalization of an area in which a commercial use is permitted;
- Establishment of objectives for the form and character of intensive residential development;
- Establishment of objectives for the form and character of commercial, industrial or multi-family residential development;
- Establishment of objectives to promote energy conservation;
- Establishment of objectives to promote water conservation;
- Establishment of objectives to promote the reduction of greenhouse gas emissions.

A Development Permit may vary or supplement the regulations of City bylaws such as the *Zoning Bylaw*, *Subdivision and Development Servicing Bylaw*, and/or *Sign Bylaw*. However, a Development Permit may not vary the permitted use or density of any parcel of land; except in the case of land for the protection of development from hazardous conditions as it relates to health, safety, or protection of property from damage.

Where a parcel is designated as more than one type of development permit area, a single development permit may be issued, provided that the guidelines for all applicable development permit areas are addressed in the development permit.

For specific procedures relating to the issuance of Development Permits refer to the City's *Development Procedures Bylaw*.

1.2.3 AREA OF APPLICATION

Development Permit Areas have been established as listed below. The **Citywide** and **Building Type Guidelines** (in Sections 2.0 and 3.0) apply citywide, unless otherwise noted. **Neighbourhood Specific** and **Environmental Guidelines** (in Sections 4.0 and 5.0) apply only to their relevant Development Permit Area(s).

- a. The following Development Permit Areas have **Citywide Guidelines** and **Building Type Guidelines** that apply:
 - i. The geographic Development Permit Areas identified on Schedule A:
 - Burquitlam-Lougheed Neighbourhoods;
 - Waterfront Village;
 - Austin Heights Neighbourhood Centre;
 - Maillardville Neighbourhood Centre;
 - Maillardville Multi-Family Residential;
 - Laval Square;
 - Lougheed/Brunette;
 - Lougheed/Schoolhouse;
 - Schoolhouse Street;
 - Allard-LeBleu.
 - ii. The geographic Development Permit Areas identified on Schedule B:
 - City Centre;
 - Windsor Gate;
 - Barnet Corridor;
 - Christmas Way;
 - Partington Creek.
 - iii. The land-use based Development Permit Areas which apply to the entire Southwest Coquitlam Area Plan area:
 - Southwest Coquitlam Commercial, Industrial and Multi-Family Residential;
 - Neighbourhood Attached Residential (Housing Choices);
 - iv. The land-use based Development Permit Area which applies to the entire Northeast Coquitlam Area Plan area:
 - Northeast Coquitlam Townhouse Residential;
 - v. The land-use based Development Permit Area which applies to the entire City:
 - Duplex;
 - vi. The land-use based Development Permit Area which applies to the entire Partington Creek Neighbourhood Plan Area:
 - Partington Creek Commercial and Multi-Family Residential
- b. Additionally, the following geographic Development Permit Areas have **Neighbourhood Specific Guidelines** that also apply:
 - Maillardville Neighbourhood Centre;
 - Maillardville Multi-Family Residential;
 - Laval Square;
 - Allard-LeBleu;
 - Waterfront Village;
 - Windsor Gate;
 - Partington Creek Neighbourhood Centre.
- c. The following Development Permit Areas have **Environmental Guidelines** that apply:
 - i. The geographic Development Permit Areas identified on Schedule A:
 - Braid Street Fill Site;
 - Scott Creek;
 - ii. The geographic Development Permit Area identified on Schedule B:
 - Unstable Slopes;
 - All lands directly adjacent to and within 200 metres of the Interface Wildfire Risk Management boundary.
 - iii. The “Watercourse Protection” Development Permit Area” which applies to:
 - All lands in the Northeast Coquitlam Area Plan within 50 metres of the top of bank of a watercourse;
 - All lands in the Austin Heights, Maillardville and Burquitlam-Lougheed Neighbourhood Plan areas within 30 metres of the top of bank of a watercourse;

1.2.4 DEVELOPMENT PERMIT GUIDELINE CATEGORIES

The Development Permit Guidelines in the following sections are organized under the categories listed below. Each category has key objectives; the Development Permit Areas and associated guidelines support the achievement of these objectives.

Categories and Objectives

- a) **Building Design:** Ensure building design establishes strong street definition, considers privacy for residents and effectively transitions building forms while promoting design excellence.
- b) **Active Frontages:** Ensure all new development fosters an active and people-focused public realm.
- c) **Site Design:** Improve site walkability while taking advantage of natural assets and topography and reducing the impact on adjacent lands.
- d) **Public Realm:** Encourage a balance between visual interest and design consistency while enhancing delight in the pedestrian environment.
- e) **Landscape Elements:** Promote landscaping that complements the public realm and provides opportunities to connect with nature.
- f) **Environmental Sustainability:** Encourage design that works to reduce greenhouse gas emissions and improve energy efficiency and conservation while minimizing disturbances to the natural ecology and supporting the ability of natural systems to function.
- g) **Parking, Loading + Servicing:** Ensure parking, loading, vehicle access and utility infrastructure do not detract from the public realm.

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Citywide Guidelines, All Developments



The Guidelines in this section apply citywide to all types of development in all Development Permit Areas.

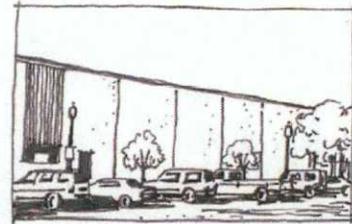
Refer to Sections 3.0 and 4.0 for specific **Building Type Guidelines** and **Neighbourhood Specific Guidelines**, where appropriate.

2.1 Building Design

2.1.1 GENERAL DESIGN

a) Avoid Blank Walls

- i. All sides of a building that are accessible to the public, residents or users of the building should receive comparable detailing and attention as other façades of the project. The creation of 'blank' walls should be avoided.
- ii. Where a blank wall is unavoidable, use the wall as an amenity by providing a feature of visual interest such as a space for public art, climbing vegetation, wall articulation, architectural features or adding outdoor furniture as part of, or against, the wall.



2.1.1 a Avoid blank walls

b) Building Materials

- i. Buildings should be designed with materials and detailing that is appropriate for the wet, west coast climate of Coquitlam and are durable and of high quality. A consistent and harmonious palette of materials and colours should be used for each development, with the following considerations:
 - Appropriate cladding materials include: brick, stone, architecturally-treated concrete, glass, and metal.
 - Low-rise buildings may also consider the use of wood, stucco, cementitious-fiber panels, or high-quality siding.
 - Where wood, stucco or concrete be used, it is to be treated to prevent staining, discoloration and the proliferation of moss.



2.1.1 b Building materials

c) Building Façade Length

- Buildings beyond the length limit in the *Zoning Bylaw*, where applicable, should provide for a substantial break or wall articulation of at least 3.0 m deep by 6.0 m wide.

d) Usable Side Yards

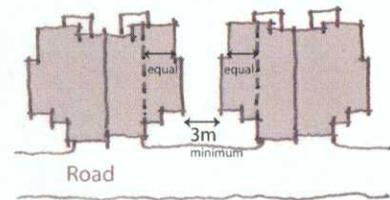
- Development shall design side yards for usable open space.

e) Single Storey Looks like Two

- Single storey, commercial buildings should have the appearance of two storeys.

f) Building Spacing

- Buildings within the same development site should be sited a minimum of 3.0 m apart.



2.1.1 f Distance between buildings

g) Live-Work/Work- Live Conversions

- Live-work units should be designed to work as a residential form, while having the flexibility to be adapted to pedestrian oriented commercial space.

h) Green Roofs

- Consider incorporating landscaped roofs, including intensive green roof systems to accommodate outdoor amenities such as sitting areas, gardening and play spaces.

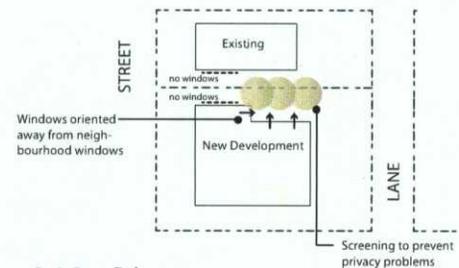


2.1.1 h Green roofs and use of rooftops

2.1.2 VIEWS, LIGHT AND NOISE

a) Privacy

- Reduce visual intrusion to residential units in the design, including consideration of unit orientation, window placement, screening, and landscaping. Use screening materials that allow views and sunlight to penetrate, especially when close to the sidewalk.



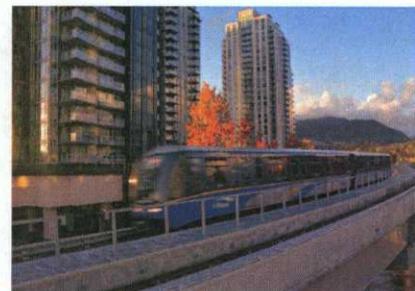
2.1.2 a Privacy

b) Minimize Noise Impacts

- Development fronting the SkyTrain guideway, and/or any Provincial Highway, shall incorporate measures to mitigate noise impacts and incorporate Canada Mortgage and Housing Corporation's (CMHC) guidelines for maximum acceptable levels of noise in dwellings, including:
 - **Sensitive site and building design:** placing noise sensitive rooms away from noise sources; locating hallways, stairwells and utility areas closer to noise sources; and using single-loaded building design;
 - **Improving building construction:** triple-glazed windows; additional wall insulation; locating vents and ducts away from noise sources; alternative ventilation systems; sound dampening or absorbing walls and cladding materials; and concrete construction; and
 - **Noise buffers:** solid glazed balconies; fences; sound absorptive landscaping; and water features.

Reference doc:

CMHC's "Qualification of the Degree of Acoustic Comfort in Multi-Family Buildings"



2.1.2 b Building near SkyTrain

c) Roof Treatments

- i. Design roof tops to hide mechanical elements and other roof-top service elements in a manner that addresses noise and visual impacts.
- ii. Consider roof top design that incorporates visual interest such as landscaping, vegetation, active areas and uses, as well as attention to stormwater management.



2.1.3 c Building tops considered

2.2 Active Frontages

2.2.1 GENERAL FRONTAGES

a) Promote Pedestrian Orientation

- The form and siting of all new development should be pedestrian oriented and face a public street, lane or walkway with minimal setback from the right-of-way.

b) Corner Development

- When located on a corner site, new development should orient frontages towards both streets (or pedestrian walkways), with active ground-floor uses. Design and finishing of entrances should respond to the visual prominence of intersections, and entrance orientation should be a mix towards both streets/walkways.



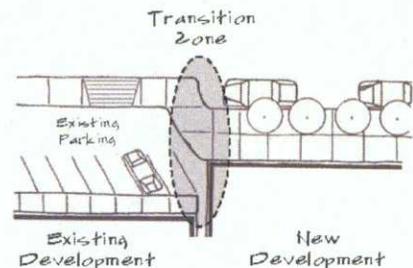
2.2.1 b Corner development

c) Enhance Rear Lanes

- Incorporate building design elements and landscaping that enhance rear lanes for pedestrian safety and establish a compatible interface with adjacent uses. Active uses facing rear lanes are encouraged.

d) Frontage Improvements

- Ensure incremental frontage improvements associated with new development provides a safe and functional transition with adjacent existing development, considering both pedestrian and vehicle access. Proponents should be expected to demonstrate transition and mitigation measures at the time of application.



2.2.1 d Sensitive transition zones

2.2.2 COMMERCIAL FRONTAGE

a) Transparency

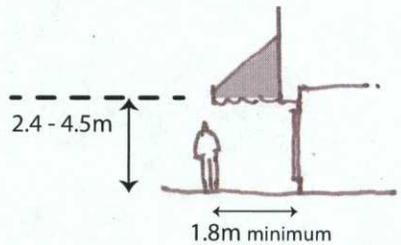
- All ground floor commercial frontages shall provide abundant transparent windows to ensure views from the street.



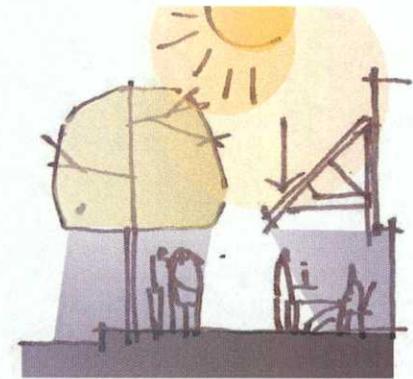
2.2.2 a Transparent shop frontage

b) Weather Protection Coverage

- Continuous weather protection (awnings/canopies) shall be provided on all buildings along **Mandatory Commercial Street Frontage**, **Optional Commercial Street Frontage**, **Secondary Active Street Frontage**, and facing Primary and Major nodes. Continuous weather protection is encouraged for all other buildings, including internal walkways.
- Weather protection shall be a minimum of 1.8 metres wide and between 2.4 - 4.5 metres high, noting that higher weather protection needs to be wider in order to provide adequate protection at ground level.



2.2.2 b Continuous weather protection



2.2.2 b Protection from the elements

- The weather protection shall be continuous, without breaks or gaps, along the entire length of the building façade. The design and location of weather protection should be coordinated with architectural elements of the building.

c) Recessed Commercial Doorways

- Commercial entrance doors should be recessed to ensure that the door swing does not intrude into the sidewalk where a frontage zone at least 1.5m wide is not provided. Glazing should be provided in the sidewalls of the recess.



2.2.2 e Individual commercial units stair-step down the slope

d) Floor Elevation of Ground-Oriented Units

- The floor elevation of all entrances and ground-level commercial space should be at the average grade of the adjoining sidewalk. Individual commercial units should be stepped with the slope.

2.2.3 RESIDENTIAL FRONTAGE

a) Street-Level Residential Character

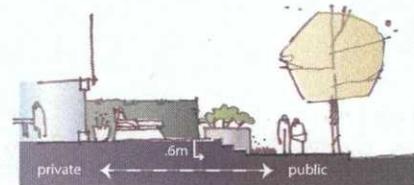
- Buildings with ground-level residential units shall be clearly identifiable, with individual entry doors located at or near the street grade. Individual residential units shall be stepped with the frontage slope. Other elements to support a residential character shall be considered such as front porches or patios with appropriately scaled stair access and landscaping elements.



2.2.3 a Street-level character with units stair-stepping down the slope

b) Slight Elevation in Residential Units

- Ground-level residential units fronting on streets or public pathways should be elevated a minimum of 0.6 metres and stepped with the slope.



2.2.3 b Living on the ground floor

2.2.4 SIGNAGE

a) Appropriate Signage

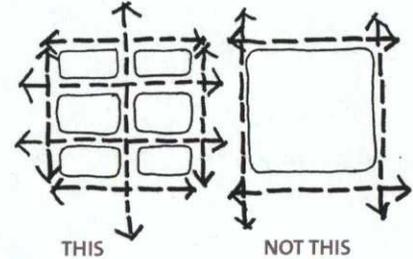
- Integrate signage with the overall design of the building and landscaping, ensuring that signs are not a dominant feature of a building façade. Unless architecturally relevant to the design of the building.

2.3 Site Design

2.3.1 INTEGRATION

a) Small Blocks

- Break up large development sites into a finer-grain block pattern with smaller sites and/or buildings, with publicly accessible streets, lanes and walkways through them.



2.3.1 a Small Blocks

b) No Land Remainders

- Site layout and design shall ensure that there are no land remainders that would be uneconomic to develop for the designated land uses.

c) Siting for Future Transportation Changes

- Site new buildings and additions according to future street requirements and/or anticipated transportation changes indicated for the area.

d) Responsiveness to Existing Development

- i. Development that is on the boundary of a land use designation where the adjacent area is of a lower density land use designation, the development shall provide an appropriate transition to the lower-density form. This transition may be accomplished by:
 - Stepping-back upper floors and/or reducing the height of the building to be progressively lower adjacent to the lower density form so that there is only a two-storey difference in height at the boundary;
 - Incorporating the upper storey into a sloping roof form to reduce the perceived mass of the building;
 - Incorporating an architectural style, detailing and/or materials that are inspired by or derived from the adjacent building;
 - Sensitive site design that locates compatible uses such as open space or amenity space adjacent to the lower density form;
 - Designing the building's façade to maintain the rhythm, pattern, and lines of the adjacent lower density building;
 - Harmonizing the setbacks and façades for portions of the building adjacent to the lower density form.
 - Providing screening or a landscaped buffer.



2.3.1 c Step down massing

e) Block Permeability

- Design the space between buildings and building side yards as a publicly accessible walkway and/or amenity space to increase the permeability of the block. Side yards should be facing with active frontages and should not be residual landscaped space.

f) Differentiate Front and Rear Orientations

- Incorporate a residential site-design that differentiates front and rear orientations in building façades and unit layout, with the rear or interior acting as the private and quiet side, with the front the more public side, except on double-fronting blocks.

g) Double Fronting Blocks

- Where a parcel fronts onto two or more streets, active uses should be provided on all street frontages.

h) Shared Access

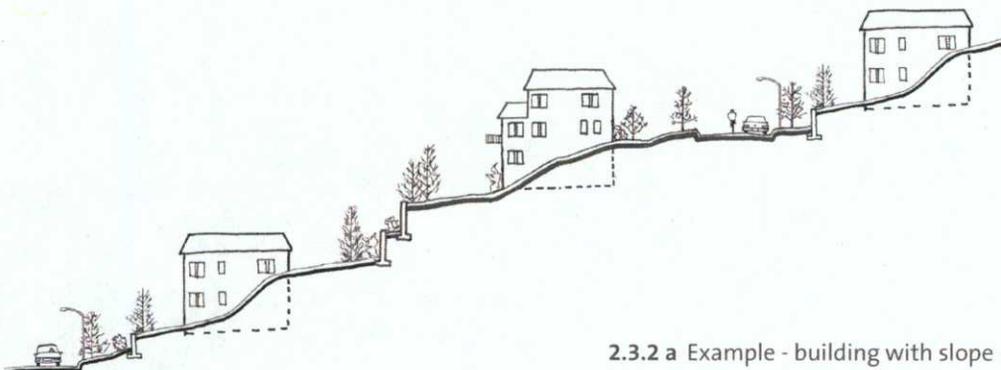
- Maintain and create new shared accesses and circulation of vehicles and pedestrians between adjacent properties, with “reciprocal access agreements” where appropriate.

2.3.2 SLOPING SITES**a) Building With the Slope**

- Consider distinctive building and site design that provides a sensitive design response to sloped sites and environmental and natural constraints.
- Build with the slope to minimize cut and fill excavations to help preserve the natural topography of the hillside;



2.3.2 a Example - building with slope



2.3.2 a Example - building with slope

- iii. To reduce grading and retaining wall requirements, set buildings into the hillside. Where retaining walls are needed, step retaining walls to follow the terrain;
- iv. Where cuts and fills are required, establish contours and gradients that resemble the naturally occurring terrain. Round out and blend slope transitions between lots and/or adjacent undisturbed areas;
- v. Create appropriately-sized terraces (for building pads that can provide usable yards, patios and common areas) when site grading. Wherever feasible, avoid the creation of large flat terraces across multiple building sites.



2.3.2 a Example - building with slope

b) Retaining Walls

- i. Create pedestrian connections, where appropriate and feasible, in locations where retaining walls create a barrier between development and streets. Where feasible, and coordinated with landscaping, also consider:
 - Utilizing ramps to improve accessibility; and
 - Incorporating bike channels to assist cyclists.
- ii. Use landscaping and plantings, including trees, to help blend retaining walls into natural surroundings
- iii. Where a retaining wall is located along any public rights-of-way, public space, or location that is highly visible, use landscaping techniques and materials to create visual interest by using:
 - Plantings, trellis or lattice-work above and below the wall;
 - Landscaped areas between walls and sidewalks/paths; and
 - Wall inlays, banding, battering and texture treatments to provide visual interest and articulation to the wall surface.
- iv. Ensure retaining walls use materials that are compatible with the principal building and/or natural surroundings. The use of brick, stone, architecturally treated concrete, and interlocking landscaping blocks is encouraged. Limited use of wood, rock and lock-block may only be considered in locations that are not highly visible. The use of wood is further limited to applications that are non-structural.
- v. Design retaining walls to permit landscaping and the maintenance of terraces, and use plants, shrubs and trees which are suitable for small spaces (preferably drought-resistant, native species) and will not create long-term maintenance issues;

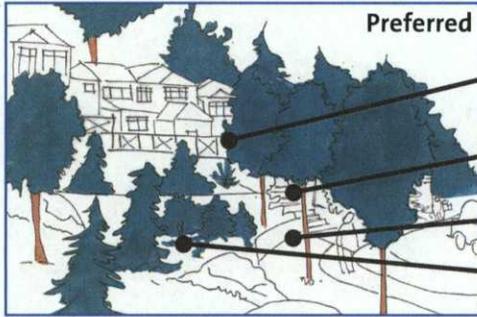


2.3.2 b Example of landscaped retaining wall

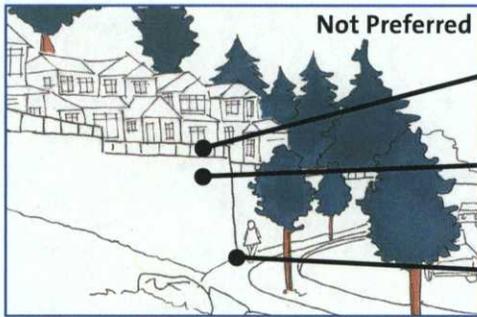


2.3.2 b Example of landscaped retaining wall

- vi. Where a retaining wall is located along any public rights-of-way, public space, or location that is highly visible, utilize transparent fencing techniques and smaller, equally-stepped retaining walls to create visual interest, reduce massing and blend the wall into the surrounding area.



- Transparent fencing reduces the scale of the wall
- Stair and ramp access connects housing with the street.
- Terraced retaining walls creates an attractive pedestrian realm
- Landscaping blends retaining walls into surroundings



- Opaque fencing increases the overall scale of the wall
- Single large retaining walls are highly visible
- Large, blank walls do not create attractive streetscapes

2.3.3 ACCESSIBILITY & SAFETY

a) Accessibility

- Incorporate best practices to improve accessibility into all projects.

b) CPTED

- Follow Crime Prevention Through Environmental Design (CPTED) best practice in the siting, configuration and design of new development.

c) Eyes on the Street

- Ensure that all private, semi-private, and public open spaces, including entries, patios, and streets, provide good visual surveillance opportunities. Consider: placement, sizing and design of windows, landscaping, and discouraging blank walls.

d) Plan for Safety

- The differences in use, ownership, and responsibilities between public, semi-private and private open spaces should be addressed during the initial planning stages with relevant approaches to safety.

e) Night-Time Lighting

- Provide good night time lighting in public, semi-private, and private open spaces.

f) Facilitate Graffiti Removal

- Materials and paints should be selected that facilitate the removal of graffiti.



2.3.3 a Provide accessibility options for all projects

Reference doc:

CMHC's "Housing for Persons with Disabilities"



2.3.3 e Lighting of open spaces provides safety

2.4 Public Realm

2.4.1 PUBLIC ART

a) Integrate Public Art into the Public Realm

- Consider both free standing and integrated public art opportunities. Where appropriate, add public art embellishments to practical elements in the public realm, such as walls, planters, benches and other seating elements, paving and steps, signage, and weather protection and lights mounted on buildings.

2.4.2 SURFACE TREATMENT

a) Sidewalk Surface Treatment

- The frontage zone between the building face and the pedestrian through zone should be designed with an integral surface treatment, that complements the rest of the sidewalk, such that it reads as a single public space, with due attention given to accessibility, functional and safety issues.

b) Limit Variety of Paving Types

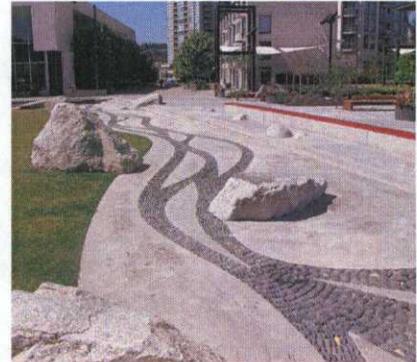
- For semi-private pedestrian surfaces, consider limiting the maximum number of different paving material types to three in a particular application. Use higher quality materials. Large areas of asphalt are discouraged.

c) Streetscape

- Refer to the neighbourhood specific Streetscape Standards for further detailed information on streetscape design, public realm and design inspiration for private space design, where applicable.
- All overhead utility wires shall be placed underground in conjunction with new development. Where undergrounding of utilities is not feasible at the time of development, pre-ducting shall be installed to permit undergrounding in the future.

Reference doc:

Public Art Policy



2.4.1 a Integrating public art

2.5 Landscape Elements

2.5.1 PEDESTRIAN ENVIRONMENT

a) Interim Pedestrian Enhancements

- Provide interim measures to enhance the pedestrian environment for subsequent phases of development, such as the use of landscaping, pathways, seating opportunities, lighting, etc.



2.5.1 a Seating and landscaping

b) Avoid Exposed Foundations

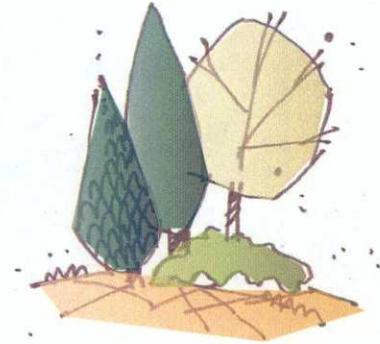
- Avoid exposed building and parking structure foundations greater than 0.6 metres. If unavoidable, design them in a manner consistent with the building finish. i.e.: siding, architecturally-treated concrete, stone or brick.

c) Fence Design

- Fences or railings should be compatible with the character of the principal building and adjacent buildings. Ensure that any fences are open and low along streets and walkways. Open fences are preferable to solid fences. Unfinished or un-surfaced concrete block or standard chain link fence are discouraged. Black-coated chain-link is allowed for wildlife management purposes.

d) High Quality Landscaping

- All new development should have a well developed landscaped yard and consider integrating usable semi-public space where fronting onto a street.



2.5.2 VEGETATION

a) Mature Tree Preservation

- The retention of mature vegetation, including existing individual and tree groupings, should be given priority in site planning. An arborist report may be required.

Reference doc:

Tree Management Bylaw

b) Tree Species and Energy Efficiency

- Emphasize deciduous over evergreen trees on the south and west sides of buildings. Evergreen trees are preferred on the north side. Tree placement should provide summer shade and winter warmth. Select species to vary texture, scale, colour and to reduce maintenance issues.

c) Low Maintenance Plants

- Prioritize drought tolerant plants native to this region in landscaping. If irrigation is necessary, drip irrigation and other water-efficient irrigation systems should be used. Plant diversity, plants that are native to the region and microclimate, and those which naturally grow together and are self-sustaining (i.e. reseed and spread without much maintenance) are preferred.



2.5.2 c Low Maintenance Landscaping

d) Perennial or Seasonal Colour

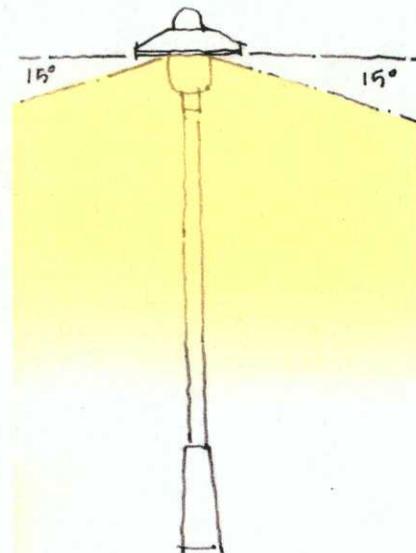
- Consider plants with perennial or seasonal colour to highlight special locations, such as courtyards, building entrances or access drives.

2.5.3 LIGHTING**a) Lighting**

- Lighting should be provided all walkways, passageways and outdoor amenity spaces.

b) Reduce Light Pollution

- Select and locate lighting in a manner that maximizes pedestrian safety while minimizing glare, light pollution or nuisance to residents or adjacent properties. Lighting should be shielded from casting light higher than a line 15 degrees below the horizontal plane as measured from the light sources, and should not cast light directly into adjacent residential windows.



2.5.3 b Minimizing light glare

2.6 Environmental Sustainability

2.6.1 NATURAL CONDITIONS

a) Support Natural Functions of Building and Site

- Use the existing site topography and natural features to inform earthworks, planting, soil, drainage and water detention that work to support the natural functions of the building and site (e.g. provide screening, windbreak, infiltration, etc.).

b) Minimize Disturbances to Geotechnical and Soil Conditions

- Respond to unique geotechnical and soil conditions and apply construction techniques that will minimize impacts to adjacent development.

2.6.2 HYDROLOGY

a) Protect Fisheries Values in Streams

- Conduct site grading, site drainage and the siting and construction of buildings and parking areas near watercourses in a manner that promotes protection of fisheries and wildlife habitat.

b) Make Creek Setbacks Attractive and Functional

- When developing adjacent to a creek, use protective fencing of a transparent and ornamental character. Consider materials such as wrought iron; stone; attractive, well-spaced, wooden fencing to enhance transparency; and glass or plexi-glass panels. Black-coated chain-link is allowed for wildlife management purposes.

c) Use Permeable Paving

- Permeable surface treatments and subsurface water storage systems, should be installed wherever possible. Consider grading hard surfaces towards landscaped areas.

Reference doc:

Rainwater Management Strategy



2.6.2 c Permeable paving

2.6.3 ENERGY

a) Reduce Urban Heat Island Effect

- Utilize tree planting, paving type, orchard parking lot design, and the use of light coloured, reflective roof surfaces or green roofs to reduce the urban heat island effect.

b) Consider Renewable Energy Sources

- Incorporate renewable energy sources (e.g. geothermal, wind, solar domestic hot water, and photovoltaic cells) and other measures to reduce energy consumption into building design.

c) Maximize Sun and Shade Opportunities

- Maximize desirable solar gain through building orientation, and use of windows and building materials, where appropriate.
- Maximize desirable shade from direct sunlight through the use of landscaping and building siting and design.

d) Maximize Natural Light and Air

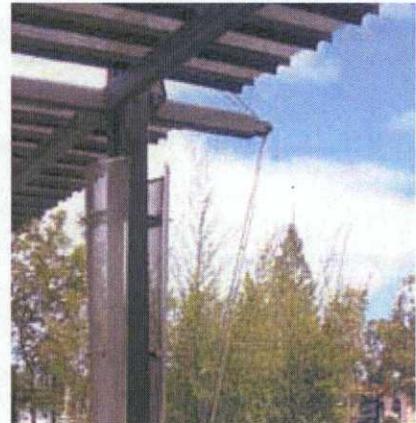
- New development should provide adequate natural light and ventilation to all units. This can be achieved through a number of means including maximizing the number of exterior walls with operable windows, and using the appropriate location and sizing of windows, skylights and the appropriate use of glazing systems and shading devices.

e) Greenhouse Gas Reduction

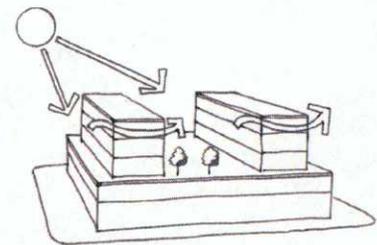
- Incorporate designs to reduce energy use, increase energy efficiency, and reduce greenhouse gas (GHG) emissions, as guided by the *Community Greenhouse Gas Reduction Strategy*. Building elements that contribute to energy efficiency include: improving building envelope performance, providing efficient lighting and appliances, well-designed ventilation and heat recovery systems, and including renewable sources in space heating and cooling systems (e.g. solar thermal, geo-exchange).

Reference doc:

Community Greenhouse Gas Reduction Strategy



2.6.3 a Shade trellis



2.6.3 d Natural light and ventilation

2.6.4 WASTE DIVERSION

a) Centralized Solid Waste Area

- Solid waste collection and storage areas are to be sited in a convenient location for the use of residents; however, they should be secure, **wildlife resistant**, and screened from view from adjacent public streets and nearby residences.

Reference doc:

Bear Aware Program

b) Provide Source-Separation Area

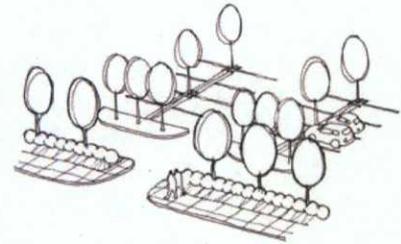
- Adequate space for operational source-separation of all solid waste (e.g. recycling and compost) should be provided, including space for collection, storage, and access for collection vehicles.

2.7 Parking and Loading

2.7.1 OFF-STREET PARKING

a) Location and Design

- i. Within all *City Centre Commercial, Neighbourhood Commercial, Transit Village Commercial, High Density Apartment Residential and Medium Density Apartment Residential* designated areas all off-street parking must be located underground. Underground or concealed parking is also encouraged for all other developments. In areas with demonstrated water table issues, concealed above-grade structured parking may be acceptable provided it is wrapped with active uses.
- ii. In areas where off-street, surface parking is allowed, it shall be located away from the street frontage, at the rear or side of the building.
- iii. Any surface parking area is required to use Landscaped Parking, including any interim surface parking proposed as part of phased development. Landscaped Parking standards for surface parking lots should include:
 - A minimum interior landscaped area of 7 percent of the total parking area;
 - At least one tree per 185 m² of paved area;
 - A minimum 2.0 metre wide walkway shall be provided through a parking area to connecting sidewalks along logical pedestrian routes;
 - A minimum 2.0 metre wide perimeter landscape buffer, including trees, should also be provided around a parking lot; and
 - Natural drainage features should be utilized.



2.7.1 a Landscaped parking lot

b) Parking Access

- i. Entrances to parking and loading should be from the lowest order street, preferably from the sides or rear of buildings. Where a lane exists access must be taken from the lane. Where a lane does not exist or cannot be provided, locate and design these accesses to minimize negative impacts through such treatments as access from a flanking street or enclosure, screening, finish materials, and landscaping.

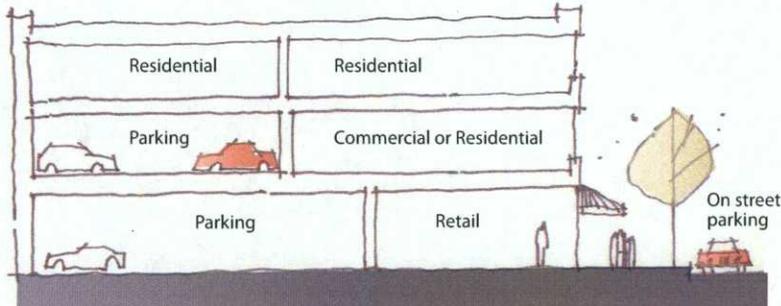


2.7.1 b Subordinate garage entrance that matches the pattern of the rest of the building

- ii. Underground or structured parking entrances should be subordinate to any pedestrian entrances and the rest of the public realm in terms of size, prominence on the streetscape, location and design emphasis. Parking entrances should be architecturally treated and should mitigate negative impacts through enclosures, screening, high quality finishes, sensitive lighting and landscaping.

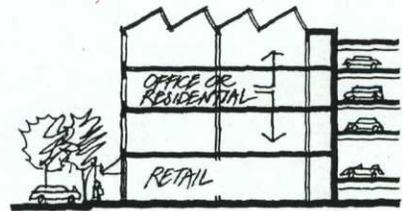
c) Facilitate Transition to Structured Parking

- The design and placement of interim surface parking lots as part of initial phases of development should be part of a comprehensive plan that includes long term structured parking, or at least does not preclude it. To facilitate this, illustrations of an ultimate development pattern including structured parking accommodation and phasing should be included in the comprehensive development plan.



d) Concealed Structured Parking

- i. In areas with demonstrated water table issues, concealed above-grade structured parking shall be separated from the street façade with active uses and activities that add to the fabric of the street (e.g. residential or commercial uses).
- ii. Above-grade structured parking directly adjacent to a rear lane is acceptable if it is appropriately wrapped by a building or screened and does not negatively impact adjacent uses.



2.7.1 d Wrapped above-grade parking

e) Minimize Impacts on Landscaping

- Design underground parking structures to minimize conflicts with landscaping features and tree pits.

2.7.2 LOADING, SERVICING + UTILITIES

a) Hide Loading, Servicing and Storage Areas from View

- i. Loading and service areas, including garbage and recycling facilities, must be concealed, preferably within a building, and accessed from a lane or rear of the building. Minor, on-street loading areas may be considered provided that they do not create conflict with the pedestrian-oriented nature of an area.
- ii. Locate outdoor storage and utilities in discrete areas, and enclosed or screened from public view.

b) Minimize Impacts from Utilities

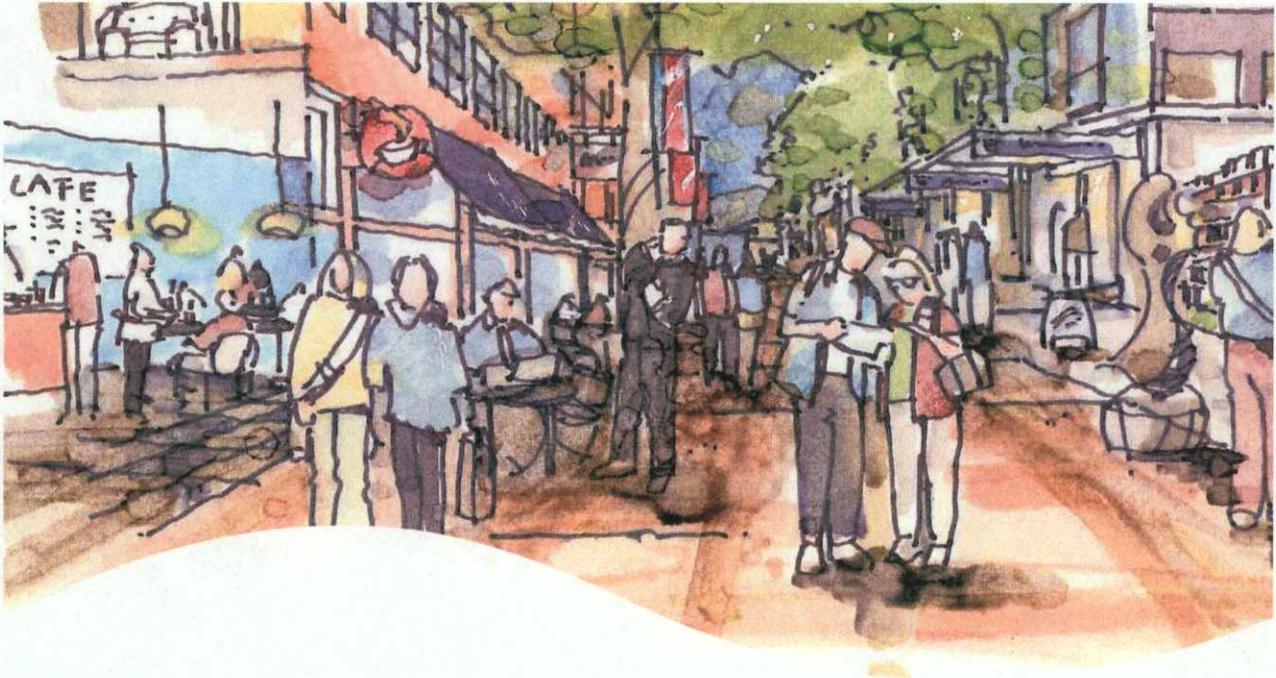
- Utilities, HVAC equipment, meters and other equipment including, but not necessarily limited to air conditioning units, fireplace vents, antennae and satellite dishes, should be located in such a manner as not to negatively impact the public realm or adjacent neighbours or be concealed from view from adjacent public streets and nearby residences.



2.7.2 a Loading and service areas located at the rear of the building

DRAFT

Building Type Guidelines



The Guidelines in this section apply to the development of specific building types in all Development Permit Areas. These guidelines can supplement the **Citywide Guidelines**, where appropriate.

Sections

- 3.1 High & Medium Density Developments
- 3.2 Townhouse & Rowhouse Developments
- 3.3 Housing Choices Developments

3.1 High & Medium Density Development Guidelines

The Guidelines in this section apply to all types of medium to high density residential/commercial and mixed-use development, such as low, medium and high density apartment residential, medium to high density commercial, and mixed-use commercial/residential buildings, in all development permit areas.

3.1.1 BUILDING DESIGN

a) Building Façade Design

- i. Buildings should be divided into three recognizable elements, a base, middle and top, with appropriate design responses to each part. This may be achieved through:
 - Cornices or other architectural features;
 - Building step backs;
 - Change in building shape;
 - Change in materials or colour;
 - Change in window/balcony design or placement.

b) Orient Wide Façade North-South

- Orient any wider portion of building, above the streetwall, north-south, minimizing the width along the east-west elevation to minimize shadowing.

c) Street End Views

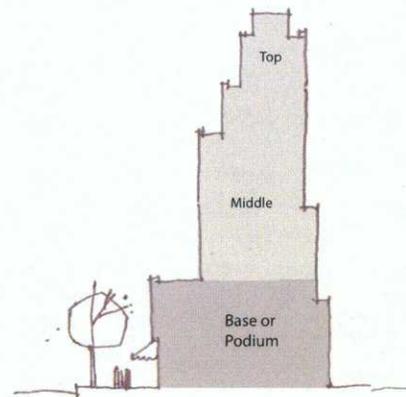
- Consider building design that provides for street end vistas. Site a unique or prominent building or portion of a building where visible at the terminus of a street or walkway.

d) Integrated Balcony Design

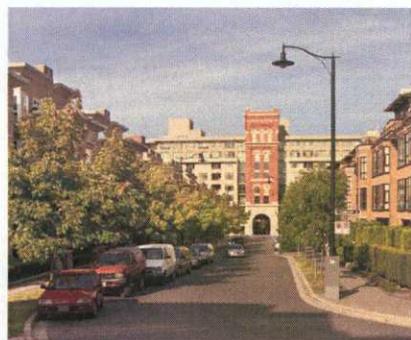
- Balconies and patio areas should be designed as an integrated part of the building. Railings should be predominantly open rail or transparent glass materials to maximize natural light penetration. Solid concrete balconies should be avoided.



3.1.1 a Articulation of building detail



3.1.1 a Building Elements



3.1.1 c Street end vistas

e) Future Flexibility

- Consider building designs that create flexibility for future uses in areas designated as *Optional Commercial Street Frontage* and *Employment Living Street Frontage*. Double height ground floor units are encouraged with floor plans designed for future conversion to commercial and/or live-work/work-live uses.

f) Minimize Shadow Impact from Development

- Development applications for buildings four storeys and above should complete a shadow impact analysis to demonstrate how building massing has been minimized to reduce shadowing impacts. Building massing should be configured so that shadows generated by a proposed development are minimized on parks, open spaces, and pedestrian-oriented commercial street frontages.

g) Building Stepback

- Buildings extending above the existing street wall set out in an area should step-back a minimum of 6 metres to allow sunlight penetration to street level and minimize wind impacts.

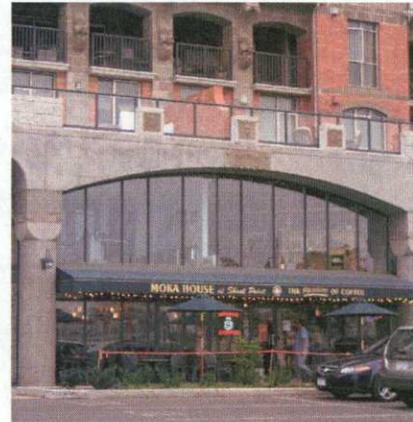
See Section 3.1.2 (a) for streetwall heights.

h) Towers

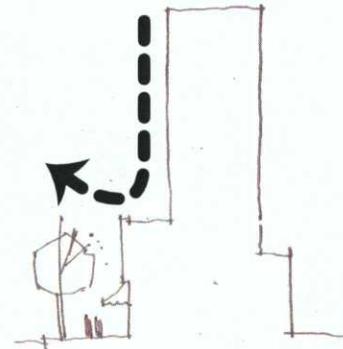
- All buildings taller than 12 storeys should have a floorplate of 700 m² or less, above the streetwall, to achieve a tall, slender shape.
- For buildings taller than 12 storeys maintain a minimum distance of 25 metres between all portions of the building above the streetwall set out for the area. (Except for Austin Heights, where the minimum distance shall be 35 metres) Where possible, space towers diagonally to each other.

i) Landmark Buildings

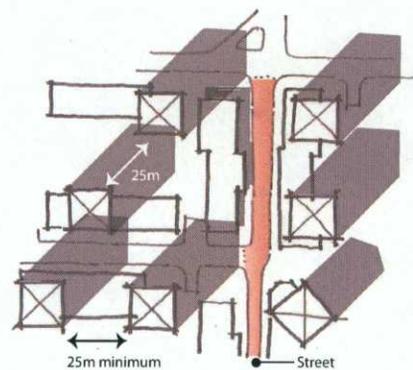
- Landmark buildings and sites shall provide a high degree of design excellence due to their prominence and location within the neighbourhood. Landmark buildings will be designed through a rigorous design-review process that includes:
 - Context responsive design that is appropriate to the location, including topography, transition and integration with adjacent buildings and street level character;



3.1.1 e Double height units



3.1.1 g Tower setback

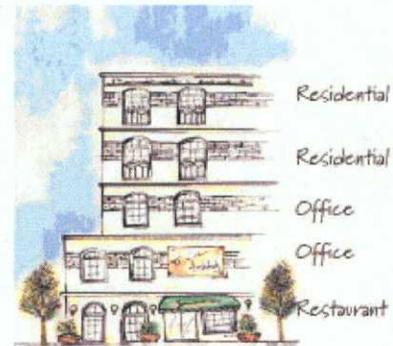


3.1.1 h An open spacing of towers

- Consideration should be made to views to and from key sites within the neighbourhood, and the impact of the building on, shadowing and wind effects;
- Landmark buildings should include attractive and publicly accessible spaces, art, lighting, and/or other amenities at the ground level, and the design of all ground-level frontages should ensure a vibrant public realm with active uses. Corner locations should be enhanced;
- The design of landmark buildings should incorporate various elements to distinguish it as unique, including a smaller or uniquely shaped floor plate, an oblique or customized building orientation, a tailored/sculpted building with appropriate proportions of each part (base, middle, top) and a distinctive profile for the upper part, appropriate lighting effects, and the use of high-quality finishes and exterior cladding materials.

j) Mixed-Use Buildings

- Commercial space should provide for ceiling heights of 4.5 metres to ensure that commercial units are flexible to support a variety of uses.
- Address any potentially disruptive impacts such as noise, vibration or odor between commercial and residential units. Measures could include:
 - Improved insulation;
 - Improved ventilation systems;
 - Sensitive unit layout and room placement, (i.e. place noise-sensitive residential rooms as far away as possible from commercial areas).
- Commercial units shall include ventilation systems, or the provision to easily install one in the future, to accommodate a commercial kitchen. Ventilation systems shall be exhausted at a location that has the least impact on both the sidewalk and residential livability.
- Consider the needs of 'back of house' functions in commercial unit layout to meet the needs of a variety of commercial tenants including restaurants and commercial kitchens. This can include individual space for, or internal building access to, solid waste disposal, loading, storage, and utility areas.



3.1.1 j Mix of uses

3.1.2 ACTIVE FRONTAGES

a) General Frontages

Streetwall height

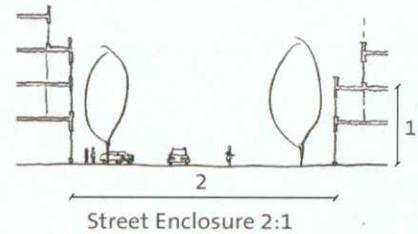
- i. A consistent streetwall (building façade) is required for all developments fronting on *Mandatory Commercial Street Frontage*, *Optional Commercial Street Frontage*, and *Secondary Active Street Frontage* as defined by the Citywide Official Community Plan and Zoning Bylaw. The minimum streetwall height is set out in the chart below.

Reference doc:
Refer to individual neighbourhood plans for street frontage types

Development Permit Areas	Minimum Streetwall Height
City Centre	3 storeys
Windsor Gate	2 storeys
Austin Heights	3 storeys
Ridgeway Avenue between Marmont St. & Nelson St. only	2 storeys
Maillardville	(see section 4.1.1.c)
Burquitlam-Lougheed	3 storeys
Partington Creek	2 storeys

Street enclosure ratio 2:1

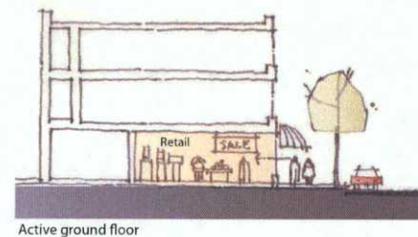
- ii. Buildings in an area without a specified streetwall height should provide for continuous street edge definition through a street width to building height ratio of 2:1 (for example, building height should be half of the distance between another building) along all existing and proposed streets and public rights-of-way.



b) Commercial Frontage

Continuous street-oriented commercial frontages

- i. Provide continuous and street oriented at-grade commercial or public uses for all ground level frontages identified as *Mandatory Commercial Street Frontage*.
- ii. Provide continuous, street-oriented commercial, residential and/or employment living uses for all ground level frontages identified as *Optional Commercial Street Frontage* and/or *Secondary Active Street Frontage*.



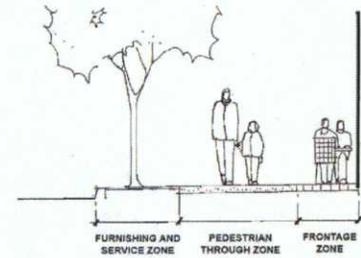
3.1.2 b Publicly oriented ground floor

Sidewalk cafés, restaurants and stores

- iii. The building design along **Mandatory Commercial Street Frontages** should enable stores, cafés and restaurants to spill onto frontage zones to provide sidewalk cafés or outdoor display, while ensuring a clear pedestrian through zone.
- iv. All buildings facing **Mandatory Commercial Street Frontage, Optional Commercial Street Frontage, Secondary Active Street Frontage**, and Primary and Major nodes shall include pedestrian-oriented lighting designed to illuminate the sidewalk. Pedestrian-oriented lighting is also encouraged for all other commercial and multiple-unit buildings.



3.1.2 b Sidewalk Café



3.1.2 b Sidewalk Zones

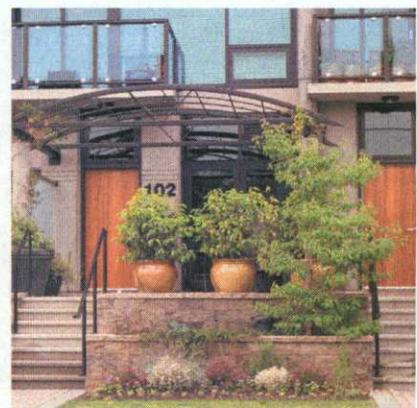
c) Residential Frontage

Continuous ground-oriented residential frontages

- i. Provide continuous and ground-oriented **individual** residential or employment living uses in the first floor of all buildings along frontages identified as **Residential Street Frontages or Employment Living Street Frontage**. Breaks in the continuous frontage can be considered for access to concealed vehicle parking or loading, provided they are carefully designed to respect the character of the street.

Residential entrances

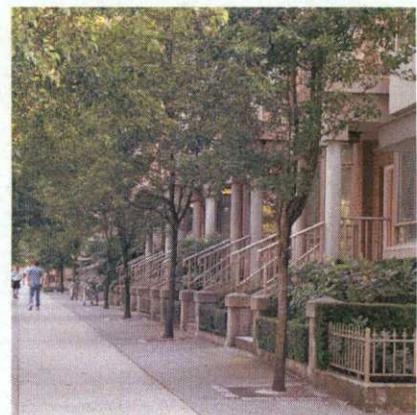
- ii. Entrances for ground-level units and building lobbies should be located near the street grade. Residential entries should be oriented to the street, separate and clearly identified. Step individual entrances with the slope.
- iii. Building lobbies should be enhanced through the use of elements such as low walls, steps, special paving, special planting features, architecturally integrated canopies projecting from the building and special lighting while ensuring direct sight lines into the lobby. Building entrances should be seen as 'punctuation' in the overall streetscape.



3.1.2 c Units addressing the street

d) Signage

- Commercial signage should be human-scaled and oriented to pedestrians not vehicles. This may be incorporated as under awning/canopy signage perpendicular to the building, where appropriate.

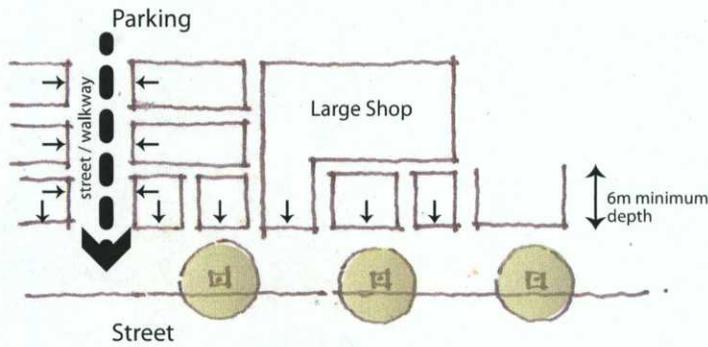


3.1.2 c Residential Entrances

3.1.3 SITE DESIGN

a) Disguise Large Tenancy Stores as Small Frontages

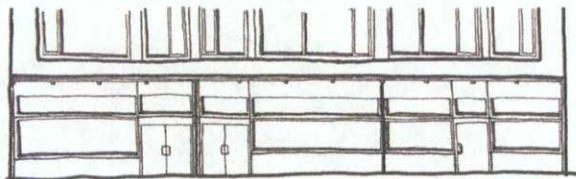
- Larger commercial tenancies should locate the majority of their area behind smaller frontages, without creating an internalized mall. Frontages should have a minimum depth of 6 metres.



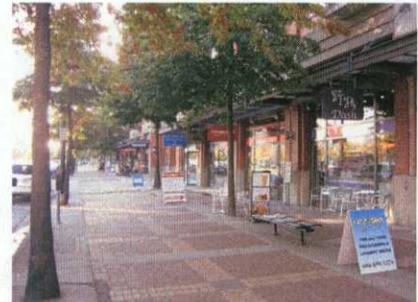
3.1.3 a Wrapping large shops

b) Small Frontages

- Buildings with grade-level commercial activity should be made walkable through being visually articulated as several smaller frontages, with individual store frontages not to exceed 10 metres. (8 metres or less preferred)



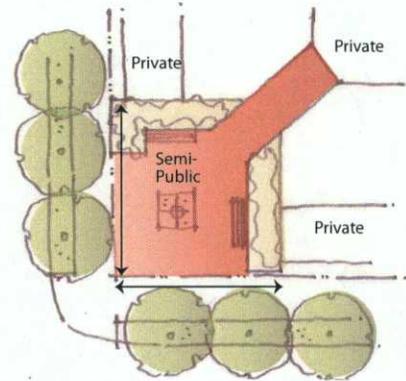
3.1.3 b Narrow shop fronts



3.1.3 b Small Frontages

c) Corners & Intersections

- i. Consider additional setbacks at corners adjacent to an intersection to provide space for a small public plaza or “outdoor room”.
- ii. Define semi-public space where a development fronts an intersection by using buildings or landscape elements on a minimum of two sides to create landscaped, outdoor rooms and spaces.
- iii. Outdoor seating for cafés and restaurants, outdoor display and other active uses are strongly encouraged in these locations, particularly for south facing corners with good solar exposure.



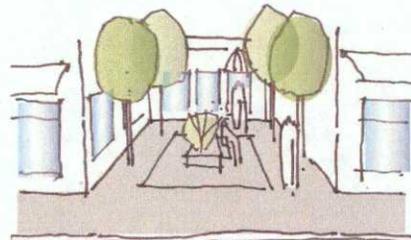
3.1.3 c Creation of outdoor rooms

d) Plaza Design

- i. Design plazas, nodes, and pedestrian mid-block walkways to ensure adequate space for access, pedestrian circulation and sunlight penetration while providing sufficient space for public gathering, café seating, outdoor display and landscaping, and shall include elements such as:
 - A hard-surface area of at least 50%;
 - Seating and other appropriate street furniture;
 - Pedestrian oriented lighting;
 - Public art, water features and wayfinding;
 - Trees and other elements to define pedestrian-scale spaces;
 - A portion with weather protection coverage to facilitate year-round use;
 - Fronting buildings shall address the space with active frontages, architecturally distinctive façades, and high-quality streetscape treatments.



3.1.3 c Small corner plaza



3.1.3 d Plaza fronted with buildings

e) Entry Courtyards

- A small sized, appropriately paved “courtyard” area at an entry to a building is encouraged, particularly on a corner. This may also take the form of recessed individual entry courtyards at street level. However, extensive paved areas which detract from the overall streetscape should be avoided.



3.1.3 d Plazas for multiple uses

f) Block Interiors

- Consider a perimeter block building design to create semi-private courtyards in the block interior.

3.2 Townhouses & Rowhouses Guidelines

The Guidelines in this section apply to attached low-rise, ground-oriented housing, such as townhouses and rowhouses in all Development Permit Areas.

3.2.1 BUILDING DESIGN

a) Unit Differentiation

Townhouse units shall be differentiated and individually expressed to avoid monotonous repetition, while still appearing as a consistent and coherent design that reflects similar design elements, including character. This can be achieved through:

- i. Articulation of the front façade;
- ii. Variation in the placement, design, and grouping of windows, entry doors, porches,
- iii. Variation in the shape of roof forms, and other architectural elements; and
- iv. Variation in building materials, colour, and design accents.



3.2.1 a Variation in façade articulation, colour, and building materials

b) Façade Design

- i. The façades of end units facing an exterior side yard are to be architecturally treated in a manner consistent with the front façade of the building. Blank walls or architecturally plain walls are not acceptable.
- ii. Detailing and materials used on front elevations should be carried around the side elevations to the mid-point of the side walls or to the nearest articulated element (i.e. a bay, indentation or chimney).
- iii. High-quality, low-maintenance exterior cladding material such as cementitious-fiber siding, brick, stone, treated wood, and/or an acceptable alternative shall be used on all street-facing façades.
- iv. Design street fronting façades using multiple types of cladding materials, architectural detailing and/or design accents to break up large flat surfaces and monotonous façades. Large expanses of uniform materials and flat monotonous façades shall be avoided.



3.2.1 b End units eliminate blank walls through continuation of architectural treatments

- v. Design and locate fireplaces and the flue so that it is encased in a chimney structure which extends past the roof line when visible from the street. No direct vent fireplaces shall be placed on a street-facing exterior wall.

c) Roof Design

- i. Developments shall break up the building mass by articulating the roofline and by incorporating features such as dormers, gables, variation of the eaves, and architectural detailing into the roof structure. Roof structures that span multiple units in a singular, monolithic form shall be avoided.
- ii. Shallow pitched roofs should have strong and visible horizontal eave lines and large overhangs. The shallower the roof pitch, the broader the overhang should be.
- iii. Flat roofs are permitted in contemporary design projects provided they have a wide overhang and are broken up with dormers, articulation and/or projections.

d) Window Design

- iv. Window placement and design should be well-ordered and competing 'feature' windows should be avoided. Windows facing public streets are to be transparent, clear-glazed, operable, and of a scale and size large enough to promote safety and security through passive surveillance.

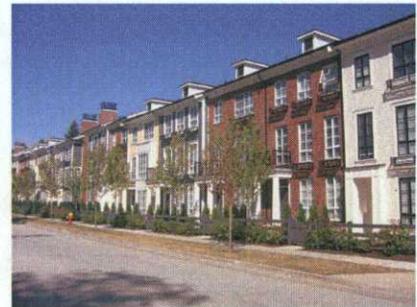
3.2.2 ACTIVE FRONTAGES

a) Unit Orientation

- i. All units should face and address a street, strata road, walkway, mews, or park with direct, unobscured access from the sidewalk or pedestrian walkway to the front porch of each unit.
- ii. Developments on corner lots or double-fronting lots should be designed with individual unit entrances facing both streets with an equal level and quality of design in detailing.



3.2.1 c Roof articulation can help to break up building massing



3.2.1 d Well ordered window placement and varied, high-quality cladding materials



3.2.2 a Direct access from public sidewalk to front entrance of units

b) Unit Entries

- i. Front entries should not be adjacent to or on the same façade as a garage door, where possible. When front doors and garage doors are unavoidable on one façade, the garage doors are to be recessed at least 1.2 metres behind the primary front of the building façade.
- ii. Units should have a second entrance to access the rear yard, where applicable.
- iii. Every unit should have a raised front porch, patio or stoop facing the street to bring a sense of scale and neighbourliness to the community. Where an entry to a unit is not clearly visible from the street, the presence and location should be visible through architectural or landscape gateway elements.

3.2.3 SITE DESIGN**a) Site Layout**

- i. Buildings shall be arranged around a central organizing feature such as a street, mews, square, park, or green.

b) Sloping Sites

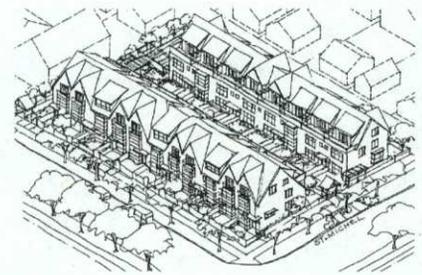
- i. Buildings should be stepped on sloping sites to reduce the height and massing of the development and reduce the need for retaining walls wherever possible. On cross slopes developments shall step the units so that the entrances are at the average grade of the adjoining sidewalk.
- ii. Design yards to be gently graded, wherever possible. Avoid high retaining walls and elevated or depressed front yards.

c) Walkways

- i. Townhouse developments will provide publically-accessible walkways through the development to provide pedestrian/cyclist connections to surrounding streets, lanes, and adjacent developments.

d) Internal Streets

- i. Private roads will include pedestrian-friendly design features and streetscape elements such as sidewalks, street trees, street furniture, traffic calming, and lighting.
- ii. All private roads, lanes, walkways, and outdoor amenity spaces shall be provided with appropriate lighting.



3.2.3 a Courtyard townhouses



3.2.3 b Buildings are stepped on sloping sites to reduce height and massing



3.2.1 d Pedestrian-friendly lane design

e) Amenity Space

- i. Common outdoor amenity space should:
 - Provide for a variety of recreation and gathering opportunities;
 - Be a focus and an organizing element of the development and not 'leftover' space, this is especially important for any units that do not have direct street frontage;
 - Have adequate natural light;
 - Provide lighting for night time use, and safety and security;
 - Be of adequate shape and size for a range of activities;
 - Provide sufficient screening, landscape, and overlook considerations for the units adjacent to the space.

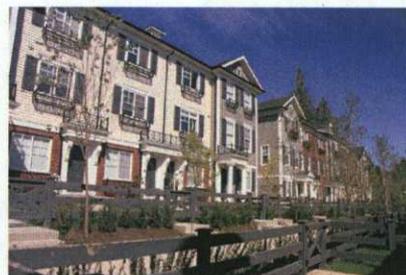


3.2.3 e On-site recreation and amenity spaces

3.2.4 LANDSCAPE ELEMENTS

a) Landscaping

- i. All open spaces shall be landscaped with a variety of trees, lawns, shrubs, flower beds, and/or other acceptable planting materials in a coordinated manner to support good landscaping practice.
- ii. A minimum of 65% of any street-facing yard should be soft landscaping, exclusive of sidewalks or pathways.
- iii. Fences or railings should be compatible with the character of the building. Fencing will be of high-quality, long-lasting material with chain-link, concrete block and wood fencing discouraged.
- iv. Low open fences, landscaping, and hedges are encouraged instead of solid fences along streets and walkways, for screening in front yards, and along exterior side yards abutting flanking streets.



3.2.4 a Visually permeable landscaping and fencing

3.2.5 PARKING AND SERVICES

a) Parking

- i. Parking should be located either behind buildings, within garages accessed from a lane/strata road or in underground parkades.
- ii. When a garage is designed to be incorporated into one of the principal buildings, it should be placed in a position that minimizes its impact on any street/lane-facing elevation(s) and be a subordinate part of the building.



3.2.5 a Unit parking in the rear

- iii. Short-term visitor parking and loading spaces should be distributed throughout a townhouse development rather than in one common area and may be located either on-street/strata road or off-street in discrete locations behind or to the side of buildings.

b) Loading and Servicing

- i. A central location for solid waste storage and collection shall be provided that has adequate screening, is wildlife resistant, and has adequate space for source separation and access by collection vehicles.

3.2.6 SUPPLEMENTAL GUIDELINES FOR STREET-ORIENTED VILLAGE HOMES (RTM-1 STREET-ORIENTED VILLAGE HOME RESIDENTIAL)

- i. No more than six dwelling units are to be attached in one building. Where intensive residential buildings are adjacent to one another, variations in the number of attached dwelling units per building are encouraged so that monotonous massing is avoided. The maximum number of dwelling units in a row may be increased to eight provided that adjacent buildings are separated by broad open areas with enhanced landscaping and/or improved pedestrian circulation routes.
- ii. The front entrances to individual dwelling units are to face directly towards the street, and should not be obscured by landscaping or architectural elements. A covered, single-storey entry porch, with a floor elevation raised at least 0.3 metres above the surrounding yard elevation, is to be provided for every dwelling unit.
- iii. All units in a Street-Oriented Village Home development shall face directly onto a public street.
- iv. Parking should be located:
 - Behind buildings within garages or on a driveway off a rear lane;
 - Underneath buildings in underground parkades.



3.2.6 i Limited number of attached dwelling units

3.2.7 SUPPLEMENTAL GUIDELINES FOR TOWNHOUSING AND CONVENTIONAL TOWNHOMES (RT-2 TOWNHOUSE RESIDENTIAL)

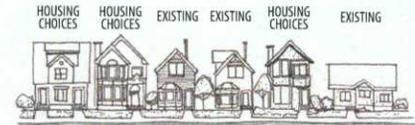
- i. No more than eight dwelling units are to be attached in one building. Where intensive residential buildings are adjacent to one another, variations in the number of attached dwelling units per building are encouraged so that monotonous massing is avoided. The maximum number of dwelling units in a row may be increased to ten provided that adjacent buildings are separated by broad open areas with enhanced landscaping and/or improved pedestrian circulation routes.
- ii. Wherever possible the front entrances to individual dwelling units are to face directly towards the street, and should not be obscured by landscaping or architectural elements. A covered, single-storey entry porch, with a floor elevation raised at least 0.3 metres above the surrounding yard elevation, is to be provided for every dwelling unit.
- iii. Parking should be located:
 - Behind buildings within garages; or
 - Underneath buildings in underground parkades.
- iv. Short-term visitor parking spaces, wherever possible, are encouraged to be located behind street fronting townhomes developments:
- v. Shared parking facilities and shared access points to these facilities are encouraged to reduce the amount and number of curb-cuts and to allow for efficient traffic circulation.



3.2.7 Street-facing front entrances

3.3 Housing Choices Guidelines

The Guidelines in this section apply to low-rise, ground-oriented housing, such as neighbourhood attached residential, quadruplex, triplex, and duplex developments, in all Development Permit Areas.

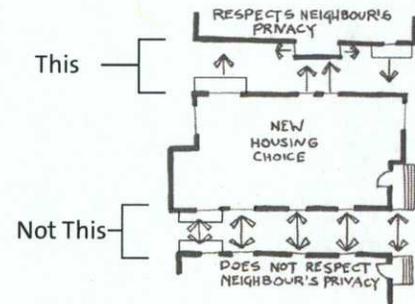


3.3.1 NEIGHBOURHOOD CHARACTER

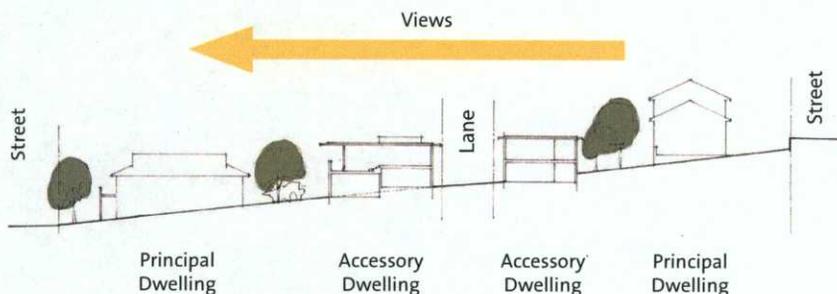
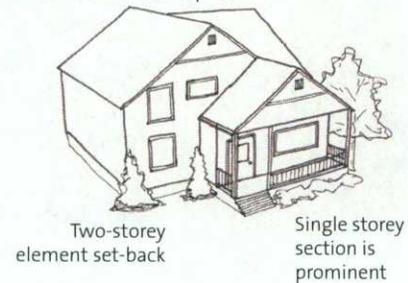
a) Neighbourhood Character

To retain and enhance the character and to respect the context of the neighbourhood, consider the following:

- i. Design and orient any residential units at the front of the lot to address the street so that it appears as a 'single-family' house with a clearly identifiable front entrance. On a corner or double-fronting site orient the building(s) to both streets.
- ii. Encourage new dwellings to minimize overlook and visual intrusion to neighbouring properties and between units within the site.
- iii. Design the roof to minimize the overall building mass, incorporating a sloped roof forms and/or articulation and variation of the eaves.
- iv. Encourage the detailing and materials used on the principal façade to continue around the side elevation to the nearest architectural break or projection or for two meters, which ever is less.
- v. Ensure that all principal and accessory buildings are designed to the same architectural style. They should reflect similar design elements, including character, cladding materials and colours.
- vi. Encourage the use of high-quality exterior cladding material such as wood, stone or brick, or acceptable alternative, on all street-facing façades.



Second story incorporated into roof to reduce impact



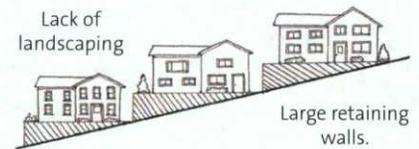
b) Neighbourhood Context - Slope and Views

To achieve the objective of respecting the neighbourhood context, consider the following:

- i. Buildings should be stepped on sloping sites to reduce the height and massing of the development and reduce the need for retaining walls wherever possible.
- ii. Design yards to be gently graded wherever possible. Avoid retaining walls and elevated or depressed front yards with entrances well above or well below street level.



This



Not This

c) Parking and Servicing

To ensure that parking and other utility functions are accommodated on site in an attractive manner, consider the following:

- i. Accommodate solid waste collection and storage in one, screened location on the site that is accessible to all units and is designed as an integral part of the development while providing convenient access for collection vehicles.
- ii. Where a functioning lane exists, provide vehicle access for all new units from the lane and locate parking in the rear of the property.
- iii. Where there is no lane, design vehicle access from the street in a way that minimizes the width of the driveway. On a corner site without lane access, design the driveway to access the rear yard from the flanking street.
- iv. When a garage is designed to be incorporated into one of the principal buildings, it should be placed in a position that minimizes its impact on the street-facing elevation(s) and be a subordinate part of the building. If placed at the front of the building, encourage it to be recessed behind the front elevation.
- v. Design the placement of all utilities, HVAC (heating, ventilation, air conditioning) equipment, meters and other machinery to ensure that they are not visible from the street or that they are appropriately screened.



3.2.2 c Minimised driveway width

d) Landscaping

To achieve high-quality landscaping and minimize hard surface treatments, consider the following:

- i. Design any large expanses of paved surfaces, such as driveways, patios or parking areas, using pervious surface materials.



3.2.2 d Variety of landscaping

- ii. Landscape all open spaces with a variety of trees, shrubs, flower beds and/or other acceptable planting materials in a professionally coordinated manner to support good landscaping practice. Native and drought tolerant plants are encouraged.
- iii. Encourage buildings to be sited and designed to retain existing mature trees.

e) Safety and Security

To achieve improved safety and increase the feeling of security in housing choice developments, consider the following:

- i. Encourage visibility from the rear yard to the lane by using a combination of enclosed and unenclosed parking.
- ii. Provide usable semi-private outdoor space, such as porches, patios and/or balconies.
- iii. Provide windows on the lane façade of a lane-facing dwelling to provide opportunities for visual surveillance.
- iv. Development design should follow Crime Prevention Through Environmental Design (CPTED) standards and practices.



3.2.2 d Lane-facing windows enhance a sense of security

3.3.2 DUPLEXES

a) Guidelines

- i. To set a minimum standard of design expectations for duplex development throughout the City, follow the Council adopted “Duplex Design Guidelines” for the construction of a new building or building addition that results in the creation of a duplex.

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Neighbourhood Specific Guidelines

4



The Guidelines in this section apply to all types of development within specific neighbourhoods and their associated development permit areas. These guidelines supplement the **Citywide Guidelines** and **Building Type Guidelines**, where appropriate.

Sections

- 4.1 Maillardville
- 4.2 Waterfront Village
- 4.3 Windsor Gate
- 4.4 Partington Creek

4.1 Maillardville

4.1.1 MAILLARDVILLE NEIGHBOURHOOD CENTRE DEVELOPMENT PERMIT AREA

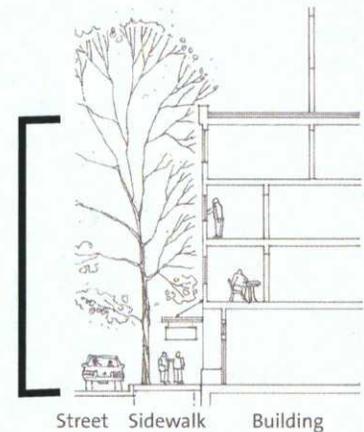
The Neighbourhood Centre Development Permit Guidelines aim to complement and reinforce Maillardville’s existing historical identity. They also strive to ensure that new buildings exhibit a consistent and authentic design character, that collectively create a distinct neighbourhood identity and sense of place in Maillardville’s Neighbourhood Centre.

These guidelines draw on existing, heritage-inspired building designs that acknowledge and celebrate the neighbourhood’s history, and also draw inspiration from the design character of buildings found within historic commercial districts, developed between the late 1800s and early 1900s, in the region. These commercial heritage buildings typically feature ‘base, middle and top’ façade designs, complete with distinct ‘storefronts’ at the building base that feature generous ground floor glazing, and use durable materials such as stone, brick and heavy wood timbers. This hybrid approach responds to the fact that new development within the Neighbourhood Centre has the potential to be of a much higher density and taller than any buildings developed there in the past, and the following guidelines provide the guidance for new building designs that reflect the local context but also help create a unique, new identity for this ‘Main Street’ area.

The primary focus of these guidelines is on the form and character of the streetwall or “podium” portions of buildings (particularly the lower 4 storeys). Portions of buildings above the streetwall or facing the rear lane should apply a complementary design outcome, that may be in a contemporary style.

The guidelines of this section, Section 2.0 Citywide Guidelines and 3.0 Building Type Guidelines, jointly apply to all types of development within the Maillardville Neighbourhood Centre Development Permit Area as shown on Schedule A.

A streetwall is the part of the building that faces the street and generally refers to the consistent ‘wall’ created by several buildings lining up at the back of the sidewalk with no or minimal setback. The streetwall helps to define the street and create an outdoor room.



Streetwall portion of a building

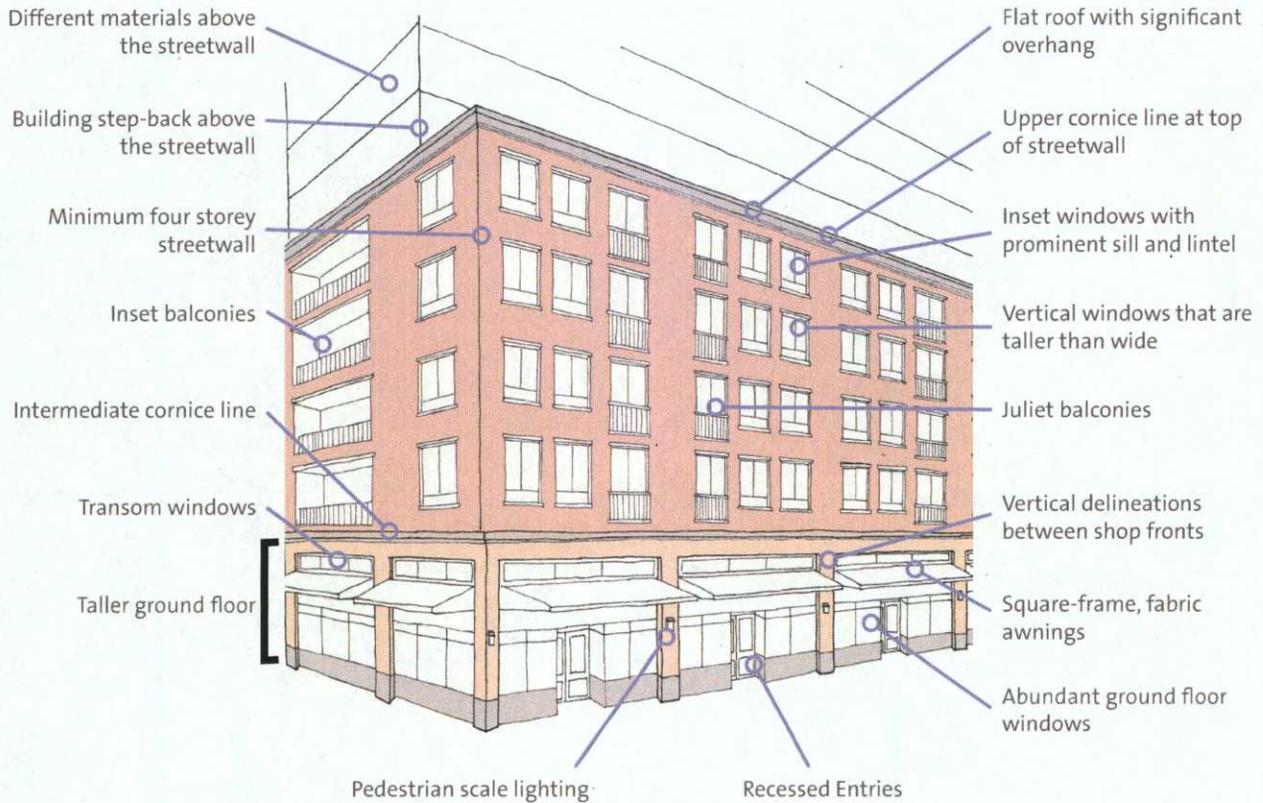


Maillardville Neighbourhood Centre Development Permit Area

4.1.1 Maillardville Neighbourhood Centre Development Permit Area - *continued*

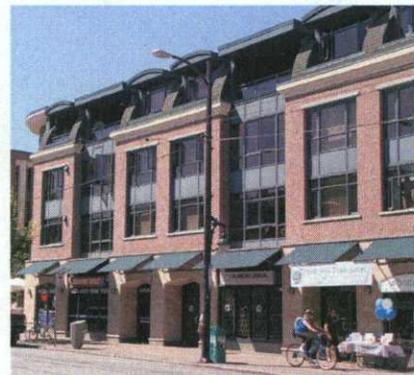
a) Maillardville Development Permit Guidelines At-A-Glance

This illustration provides a demonstration of the potential application of the Maillardville Neighbourhood Centre Development Permit Guidelines. It is not intended to represent a specific architectural style.



b) Building Design

- i. Design street fronting façades with a distinct base, middle and top with unique but complementary design responses for each.



Demonstrates building design with distinct base, middle and top.

4.1.1 Maillardville Neighbourhood Centre Development Permit Area - *continued*

Building Façade Design – supplemental to Building Type Guideline 3.1.1 a)

- ii. Require the ground floor level of streetwall buildings to incorporate the following for areas designated *Mandatory Commercial Street Frontage* and *Optional Commercial Street Frontage*:
 1. Natural, regular coarse granite, wood and heavy timbers as the primary finishing material. Brick, cementitious boards or panels, and/or traditionally-styled cast stone may be considered as an alternative to wood or as a secondary material. Aluminum, spandrel panels and vinyl should be avoided.
 2. A continuous, intermediate cornice at the uppermost edge of the ground floor level. The height of this cornice should align with the height of the cornices on neighbouring buildings.
 3. Sufficient height to accommodate transom windows, a signband strip and awnings above the entry doors and windows at the ground floor level yet below the intermediate cornice.
 4. Wood as the preferred material for window and door systems. In circumstances where other materials, such as aluminum are used, window and door systems should be anodized in dark colours.

- iii. Ensure storeys above the ground floor level of streetwall buildings include the following for areas designated *Mandatory Commercial Street Frontage* and *Optional Commercial Street Frontage*. These guidelines also apply to the entire façade of all streetwall buildings for areas designated *Residential Street Frontage*:
 1. A greater proportion of solid-wall-to-window-ratio;
 2. Window placements that form a consistent rhythm across the façade. Windows may be grouped to maximize daylight to interior spaces, provided the grouping respects the spacing rhythm;
 3. Windows that have a vertical orientation (more tall than wide), are divided into a minimum of two sections by a mullion and include a distinct sill and lintel;
 4. Windows that are wood-framed or metal (anodized in dark colours);



Demonstrates an intermediate cornice and brick and stone as primary materials



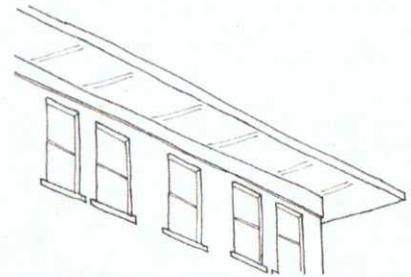
Demonstrates inset windows with dark frames that are oriented vertically grouped into a consistent rhythm

4.1.1 Maillardville Neighbourhood Centre Development Permit Area - *continued*

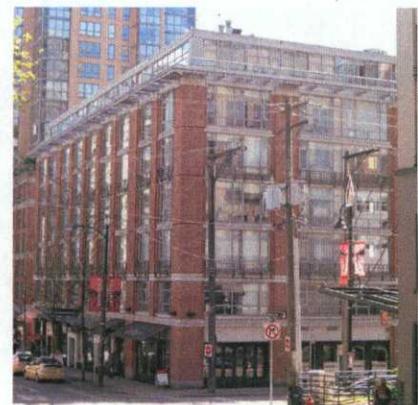
5. Windows that are inset from the building face (approximately 0.10 m or more) to provide texture, depth and shadow; and
 6. Wood as the primary finishing material. Cementitious boards or panels, brick, and/or traditionally-styled cast stone may be considered as an alternative to wood or as a secondary material. Aluminum and vinyl should be avoided. Stucco is only appropriate as a secondary material.
- iv. Incorporate transom windows, heavy wood doors and trims, heritage-inspired hardware, and contrasting colours for residential entries.
 - v. Consider building designs that integrate either mansard or flat roof styles at the top of all streetwall buildings.
 - Mansard roof style: incorporate windows or dormers that create usable space immediately behind the windows or dormers. If it is technically impossible to create habitable space, create the appearance of usable space behind the windows. Consideration should be given to material, colour and tone that complements the heritage-inspired style of the building.
 - Flat roof style: incorporate generous overhangs (approximately 1m in depth) or a substantial cornice. Soffits in overhangs should have a solid, high quality finish and the ribbing or seams should occur parallel to the face of the building.
 - vi. The primary building materials for portions of buildings above the streetwall should be distinct from those applied to the façade of the streetwall portion of the building.
 - vii. Incorporate detailing materials such as zinc for flashings and rain water leaders, steel fretwork – simple, wrought iron-like design – for planter boxes on residential windows and mounting hardware for light fixtures and commercial signage, and stone or precast concrete sills and lintels.



Demonstrates a contemporary mansard roof with dormers



Demonstrates a generous overhang with a flat roof style



Demonstrates different materials above the streetwall and metal fretwork for planter boxes.

4.1.1 Maillardville Neighbourhood Centre Development Permit Area - *continued*

Integrated Balcony Design – supplemental to Building Type Guideline 3.1.1 d)

- viii. Where balconies are included, encourage recessed and ‘Juliette’ balconies along the streetwall for areas designated as *Mandatory Commercial Street Frontage* and *Optional Commercial Street Frontage*. Steel fretwork – simple, wrought iron-like design – should be the primary material for railings. Projecting balconies are discouraged.

Future Flexibility – supplemental to Building Type Guideline 3.1.1 e)

- ix. Incorporate double-height units on the ground floor level with ceiling heights of 4.5m – 5m for areas designated as *Mandatory Commercial Street Frontage* and *Optional Commercial Street Frontage*.

c) Active Frontages

Transparency – supplemental to Citywide Guideline 2.2.2 a)

- i. Design ground floor commercial frontages that maximize transparent glazing, including recesses for entries. Mirrored and spandrel glazing is discouraged.

Weather Protection Coverage – supplemental to Citywide Guideline 2.2.2 b)

- ii. Provide simple, square-framed, canvas awnings as the primary method of weather protection along ground floor commercial frontage. Retractable awnings are encouraged in areas of outdoor seating.

Recessed Commercial Doors – supplemental to Citywide Guideline 2.2.2 d)

- iii. Encourage ground floor commercial frontage that includes recessed entries, to ensure the door swing does not intrude into the sidewalk movement zone, up to 2.0 metres in width. However, large entry courtyards are discouraged.

General Frontages – supplemental to Building Type Guideline 3.1.2 a)

- iv. Provide a consistent streetwall (building façade) for all developments fronting onto areas designated as *Mandatory Commercial Street Frontage*, *Optional Commercial Street Frontage* and *Residential Street Frontage* as defined by the Maillardville Neighbourhood Plan. The minimum streetwall height for specific frontage types is set out in the chart below.

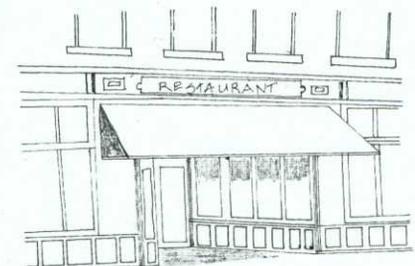
Street Frontage Type	Minimum Streetwall Height
Mandatory Commercial Street Frontage	4 storeys
Optional Commercial Street Frontage	3 storeys
Residential Street Frontage	3 storeys



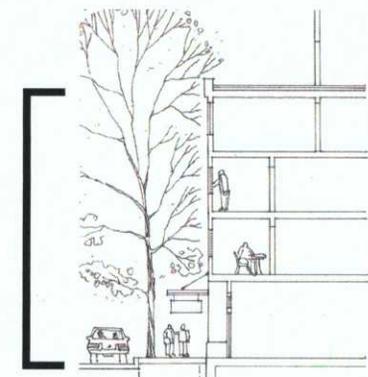
Demonstrates inset balconies



Demonstrates double-height ground floor with transom windows, square-framed, fabric awnings and abundant windows



Demonstrates recessed commercial entrance, transom windows, awning, abundant windows and sign band.



Street Sidewalk Building
Streetwall portion of a building

4.1.1 Maillardville Neighbourhood Centre Development Permit Area - *continued*

Commercial Frontage – supplemental to Building Type Guideline 3.1.2 b)

- v. Incorporate a continuous and street oriented at-grade commercial use for all ground level frontages for areas identified as *Mandatory Commercial Street Frontage*.
- vi. Encourage continuous and street oriented commercial and/or employment living uses for all ground level frontages for areas identified as *Optional Commercial Street Frontage*.

Signage – supplemental to Citywide Guideline 2.2.4 a)

- vii. Provide commercial signage that fits with the overall design and materials of the building, that are directly lit (no back lit box signs), that applies imagery and font styles that are evocative of the business that is being advertised, uses steel fretwork (simple, wrought iron-like design) as mounting hardware and integrates other complementary details and materials such as wood. Preferred signage typed include:
 - Awning signs
 - Projecting signs (usually with a horizontal orientation and incorporated under awnings or between the ground floor level cornice and above the transom windows) and
 - Fascia or shop-front signband signs (horizontal orientation and incorporated below the ground floor level cornice and above the transom windows)

Residential Frontage – supplemental to Building Type Guideline 3.1.2 c)

- viii. Provide a continuous and ground-oriented residential use in the first floor of all buildings along frontages for areas identified as *Residential Street Frontage*. Breaks in the continuous frontage can be considered for access, where permitted by the City, to concealed vehicle parking or loading areas, lobby/building entrances and amenity spaces, provided they are carefully designed to respect the character of the street.
- ix. Further to guidelines v), vi), and vii), above small entry courtyards are permitted along the north side of Brunette Avenue provided that all enclosed sides of the courtyard provide active frontages of the usage type required for that portion of the street.

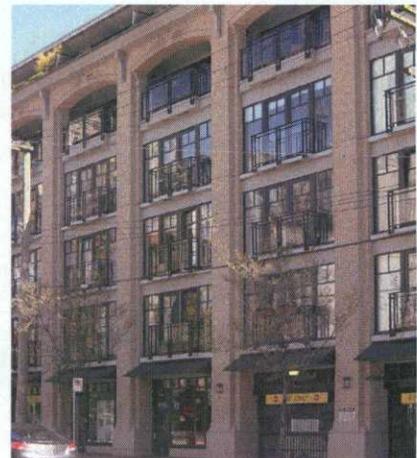
d) Site Design

Small Frontage – supplemental to Building Type Guideline 3.1.3 b)

- i. Include vertical delineations along the building façade that provide for a strong architectural detail, particularly between individual 'storefronts', where feasible.



Demonstrates a strong consistent streetwall across multiple buildings



Demonstrates a building façade broken up with strong vertical delineations

4.1.1 Maillardville Neighbourhood Centre Development Permit Area - *continued*

Building Façade Length – supplemental to Citywide Guideline 3.1.3 b)

- ii. Consider building design that breaks up the massing of large streetwall buildings into a small-scale vertical pattern that contributes to a 'fine-grained' streetscape character for areas designated as *Mandatory Commercial Street Frontage* and *Employment Living Street Frontage*. The pattern or rhythm should reflect typical 'storefronts' widths on the ground floor level and in no case should exceed 10 metres.

e) Public Realm

Public Art – supplemental to Citywide Guideline 2.4.1 a)

- i. Use the neighbourhood's local history, its pioneers, features of the natural environment and its ethnic heritage as sources of inspiration for public art. Public art in Maillardville can be achieved through stand-alone installations or embellishments of elements such as retaining structures, fences, planters, benches and other seating elements, paving inlays, signage, weather protection and lighting.



Demonstrates decorative metal fretwork

Streetscape Guidelines

- ii. Apply the Maillardville Streetscape Guidelines for all required frontage improvements (perimeter works and services).

f) Landscape Elements

Fence Design – supplemental to Citywide Guideline 2.5.1 c)

- i. Employ steel fretwork – simple, wrought iron-like design – as the primary material for fences and gates. A semi-gloss black finish is preferred.

High Quality Landscaping – supplemental to Citywide Guideline 2.5.1 d)

- ii. Consider massed plantings of lavender as well as hedges and shrub plantings that can be easily manicured.

Perennial or Seasonal Colour – supplemental to Citywide Guideline 2.5.2 d)

- iii. Provide planters, window boxes and gardening spaces for annual and seasonal plantings. Use steel fretwork – simple, wrought iron-like design – where practical. A semi-gloss black finish is preferred.

Lighting – supplemental to Citywide Guideline 2.5.3 a)

- iv. Incorporate building-mounted pedestrian-scale lights in public and semi-public areas, particularly along active frontages. Use steel fretwork – simple, wrought iron-like design – as the primary material for mounting brackets. A semi-gloss black finish is preferred.



Demonstrates black steel window boxes



Demonstrates building mounted pedestrian-scaled lighting

4.1.2 HERITAGE CHARACTER AREAS - LAVAL SQUARE AND ALLARD-LEBLEU - DEVELOPMENT PERMIT AREA

These guidelines aim to ensure a consistent design approach for all future development in Laval Square and Allard-LeBleu that reflects the existing heritage character and the design of the homes built in the early 1900s in these distinct settlement areas. This is primarily represented in the style, materials, structure, detailing, design, and architecture of the homes, some of which were built by French Canadian settlers that worked at Fraser Mills, and used lumber milled at the Mill to construct their homes.

The guidelines in this Section, Section 2.0 Citywide Guidelines and 3.0 Building Type Guidelines, jointly apply to all types of development within Heritage Character Areas - Laval Square and Allard-LeBleu - Development Permit areas as shown on Schedule A.

Design guidelines for rehabilitating heritage buildings are addressed through the use of a Heritage Conservation Plan for individual buildings, as part of a Heritage Revitalization Agreement (HRA) or Heritage Alteration Permit. New development on a lot where heritage buildings are located will also apply the guidelines of this section unless otherwise stipulated in a Heritage Conservation Plan.

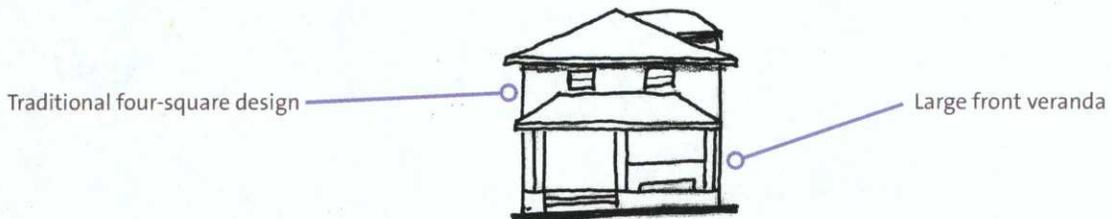
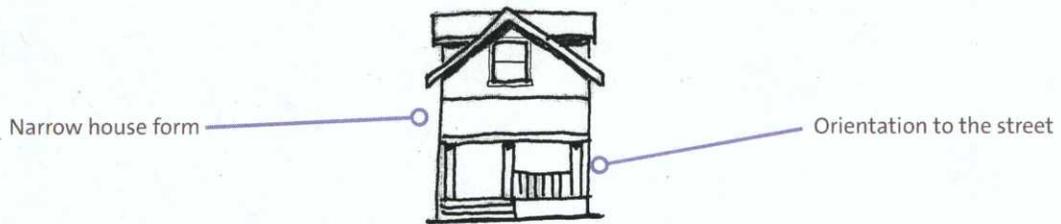
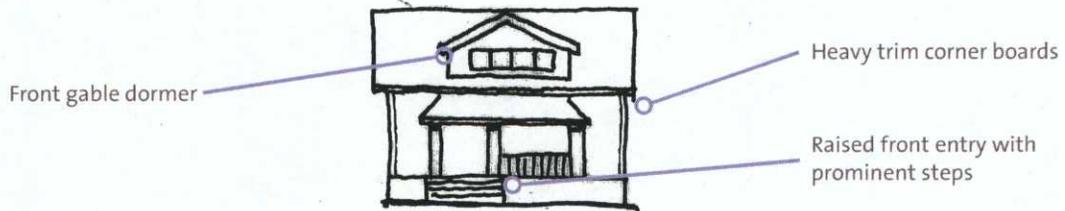


Laval Square and Allard-LeBleu
Development Permit Areas

4.1.2 HERITAGE CHARACTER AREAS - LAVAL SQUARE AND ALLARD-LEBLEU - DEVELOPMENT PERMIT AREA - CONTINUED

a) Laval Square and Allard-LeBleu Development Permit Guidelines At-A-Glance

These illustrations provide examples of traditional house designs found in Laval Square and Allard-LeBleu and serve as a demonstration of the potential application of the Development Permit Guidelines. These are not intended to represent a specific architectural style.



4.1.2 HERITAGE CHARACTER AREAS - LAVAL SQUARE AND ALLARD-LEBLEU - DEVELOPMENT PERMIT AREA - CONTINUED

a) Building Design

Massing and Street Rhythm – supplemental to Building Type Guideline 3.2.1 a) & 3.2.2 a) i.

- i. Maintain the scale and rhythm of the existing block and lot patterns. Lot consolidation is discouraged except in those circumstances where the scale and rhythm is reflected in the building form and is continued on the consolidated lands.
- ii. To help maintain the existing residential character of smaller individual buildings, provide for a maximum of four or fewer dwelling units in one building for Low Density Apartment and Urban Townhousing areas.



Demonstrates smaller individual houses stair-stepping with the slope

Orientation to the Street – supplemental to Building Type Guideline 3.2.1 a) v. & c) ii.

- iii. Incorporate a main entrance in new development that is oriented to and visible from the street and incorporate prominent steps leading up to a raised or elevated porch or veranda, while accommodating universal accessibility, along the street fronting façade of buildings.
- iv. Consider, where possible, a shared porch or veranda for new development that has multiple entrances along the street facing façade. When located on a corner site, new development should orient entrances to both streets.



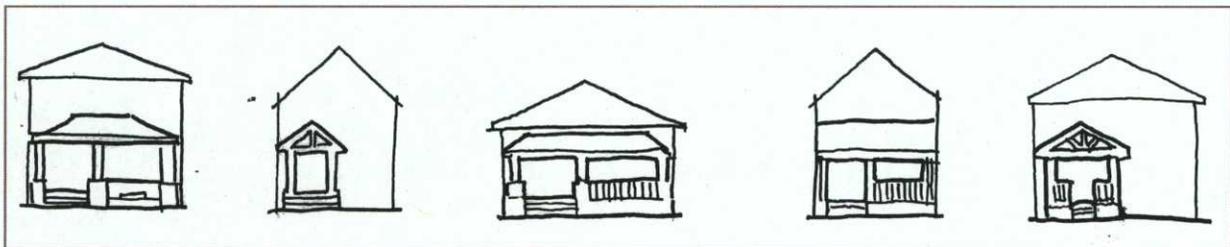
Demonstrates corner lot orientation to the street and a large porch

Street Façades – supplemental to Building Type Guideline 3.2.1 a)

- v. Design new development that provides a greater proportion of solid-wall-to-window ratio on all street facing façades.

Unit Amenity – supplemental to Building Type Guideline 3.2.1 a) & 3.2.2 a)

- vi. Provide each individual dwelling unit with direct access to outdoor yard space, roof terrace or balcony that is screened to provide privacy from neighbours.



Demonstrates different porches and verandas

4.1.2 HERITAGE CHARACTER AREAS - LAVAL SQUARE AND ALLARD-LEBLEU - DEVELOPMENT PERMIT AREA - CONTINUED

Windows – supplemental to Building Type Guideline 3.2.1 a) iv.

- vii. Consider building design for all street facing façades that provide windows:
1. whose placement forms a consistent rhythm across the façade. Windows may be grouped to maximize daylight to interior spaces, provided the grouping respects the spacing rhythm;
 2. that have a vertical orientation (more tall than wide), are divided into a minimum of two sections by a mullion and include a distinct sill, lintel and heavy trim; and
 3. that are wood-framed or metal anodized in dark colours.

Roof Forms – supplemental to Building Type Guideline 3.2.1 a) iii & 3.2.2 a) iii.

- viii. Incorporate gable (with a 1:1 roof pitch), hipped (with a 6 in 12 roof pitch or steeper) or gambrel roof forms.

Cladding Materials and Detailing – supplemental to Building Type Guideline 3.2.1 a) vi & 3.2.2 a) iv and vi.

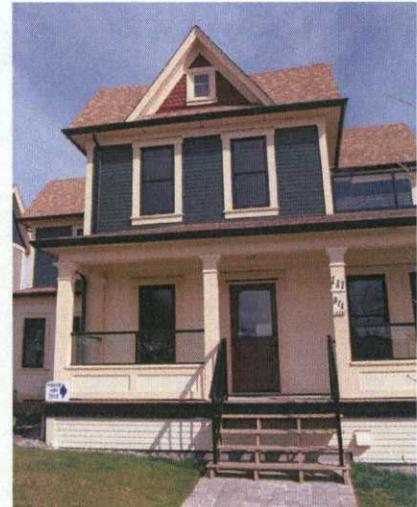
- ix. Incorporate primary building materials for new development that consist of wood siding or cementitious siding or panels, particularly for street-oriented portions of the building. Stucco, brick or stone is only appropriate as a secondary material. Vinyl or metal siding is not appropriate. Consideration should also be given to detailing, such as but not limited to (refer to the Maillardville Heritage Inventory for a diversity of design inspirations):
- heavy trim around doors and cornerboards
 - modestly decorated vergeboards
 - ornamental shingles within gables
 - columns with capitals
 - colours that generally conform to a 'heritage palette', consistent with early neighbourhood history

Landscaping – supplemental to Building Type Guideline 3.2.2 d) ii.

- x. Landscaping should be formal in design, symmetrical in character and include regularly-spaced hedges and shrub plantings that can be easily manicured, as well as groupings of flowers.

Streetscape Guidelines

- xi. Apply the Maillardville Streetscape Guidelines for all required frontage improvements (perimeter works and services).



Demonstrates wood-framed windows with dark trim that have a vertical orientation



Demonstrates a steep pitched gable roof and traditional styled wood siding with heavy trim

4.1.3 MAILLARDVILLE MULTI-FAMILY RESIDENTIAL DEVELOPMENT PERMIT AREA

These guidelines aim to ensure a consistent design approach to all future multi-family development that responds to Maillardville's history. These guidelines encourage development to include 'heritage-influence' design elements, cladding materials, windows treatments and other finishes that are informed by local historic homes, some of which were built by French Canadian Settlers. These guidelines also integrate design treatments that have been applied to multi-family developments constructed in recent decades and account for the different scale and building typology relative to heritage homes in the area..

The guidelines in this section, of Section 2.0 Citywide Guidelines and of 3.0 Building Type Guidelines, jointly apply to all types of multi-family apartment and townhouse development within the entire Maillardville Neighbourhood Plan Area.

Design guidelines for rehabilitating heritage buildings are addressed through a Heritage Conservation Plan for buildings as part of a Heritage Revitalization Agreement (HRA) or Heritage Alteration Permit. New development on a lot where heritage buildings are located will also apply the guidelines of this section unless otherwise stipulated in a Heritage Conservation Plan.

a) Building Design

Building Façade Design – supplemental to Building Type Guideline 3.1.1 a)

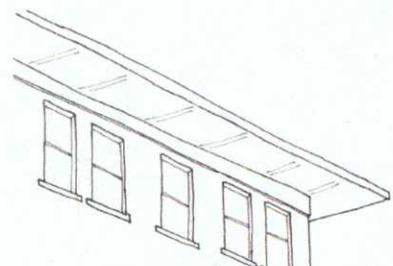
- i. Incorporate transom windows, heavy wood doors and trims, significant hardware, and contrasting colours for residential entries on all street facing façades.
- ii. Consider building design that integrates either mansard or flat roof styles:
 - » Mansard roof style: incorporate windows or dormers that create usable or the appearance of usable space immediately behind the windows or dormers at the level on which they appear. Consideration should be given to material, colour and tone that complements the heritage-inspired style of the building.
 - » Gable roof style: Incorporate a 'stand tall' or steep-pitched rather than 'sheepish' low lying profile.
 - » Flat roof style: incorporate generous overhangs (approximately 1m in depth). Soffits in overhangs should have a solid, high quality finish and the ribbing or seams should occur parallel to the face of the building.



Demonstrates 'heritage-influenced' multi-family buildings



Demonstrates a contemporary mansard roof with dormers



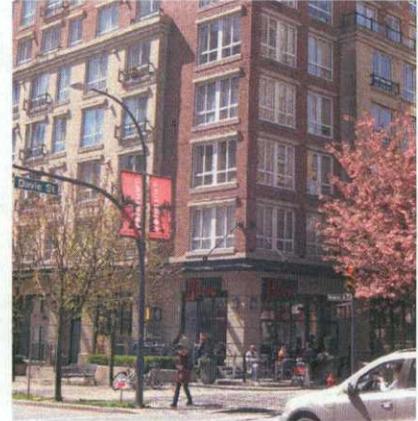
Demonstrates a generous overhang with a flat roof style

4.1.3 MAILLARDVILLE MULTI-FAMILY RESIDENTIAL DEVELOPMENT PERMIT AREA - *CONTINUED*

- iii. Incorporate detailing materials such as zinc for flashings and rain water leaders, steel fretwork for planter boxes at residential windows and mounting hardware for light fixtures, and stone or precast concrete sills and lintels.

Windows – supplemental to Building Type Guideline 3.2.1 a) iv.

- iv. Consider building design for all street facing façades that provide windows:
 1. whose placement forms a consistent rhythm across the façade. Windows may be grouped to maximize daylight to interior spaces, provided the grouping respects the spacing rhythm;
 2. that have a vertical orientation (more tall than wide), are divided into a minimum of two sections by a mullion and include a distinct sill and lintel; and
 3. that are inset from the building face to provide texture, depth and shadow.



Demonstrates inset windows with a vertical orientation and a consistent rhythm



Demonstrates a consistent rhythm across the façade and vertical orientation of the windows

Cladding Materials and Detailing – supplemental to Building Type Guideline 3.2.1 a) vi & 3.2.2 a) iv and vi.

- v. Incorporate primary building materials for new development that consist of wood siding or cementitious siding or panels. Stucco, brick or regular coursed stone is only appropriate as a secondary material. Vinyl or metal siding is discouraged.

4.1.3 MAILLARDVILLE MULTI-FAMILY RESIDENTIAL DEVELOPMENT PERMIT AREA - *CONTINUED*

b) Landscape Elements

Fence Design – supplemental to Citywide Guideline 2.5.1 c)

- vi. Employ steel fretwork – simple, wrought iron-like design – as the primary material for fences and gates. A semi-gloss black finish is preferred.

High Quality Landscaping – supplemental to Citywide Guideline 2.5.1 d)

- vii. Consider massed plantings of lavender as well as hedges and shrub plantings that can be easily manicured.

Perennial or Seasonal Colour – supplemental to Citywide Guideline 2.5.2 d)

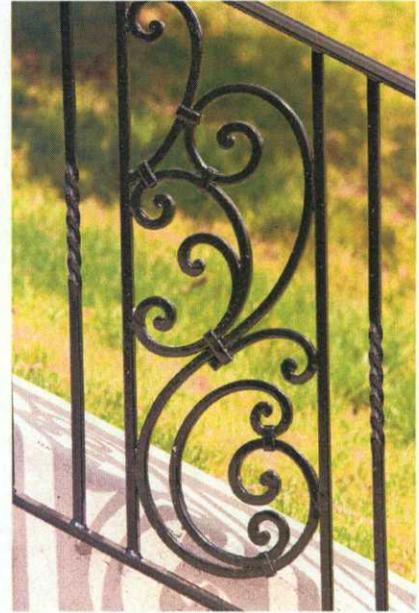
- viii. Provide planters, window boxes and gardening spaces for annual and seasonal plantings. Use steel fretwork – simple, wrought iron-like design – where practical.

Lighting – supplemental to Citywide Guideline 2.5.3 a)

- ix. Incorporate building-mounted pedestrian-scale lights in public and semi-public areas, particularly along active frontages. Use steel fretwork – simple, wrought iron-like design – as the primary material for mounting brackets.

Streetscape Guidelines

- x. Apply the Maillardville Streetscape Guidelines for all required frontage improvements (perimeter works and services).



Demonstrates decorative metal fretwork

4.2 Waterfront Village

4.2.1 WATERFRONT VILLAGE DEVELOPMENT PERMIT AREA

The guidelines in this section apply to all types of development within the Waterfront Village Development Permit Area as shown on Schedule A.

- i. The design of the built form shall be guided by the intent of the eight pillars of sustainability as contemplated in the Waterfront Village Centre Neighbourhood Plan and Development Permit applications will be accompanied by a completed Sustainability Checklist as provided in Schedule M of the Plan.
- ii. New development shall be guided by the Waterfront Village Centre Heritage Interpretive Program and the detailed Design Guidelines that are to be completed prior to the issuance of any Development Permits in this Development Permit Area.
- iii. The siting and configuration of new development sites shall be generally consistent with Schedule C - Illustrative Concept in the Waterfront Village Centre Neighbourhood Plan to provide a circulation and parks and open space network that is interconnected and encourages non-motorized transportation modes.
- iv. Transitional spaces will be used to ensure a sound relationship between different land uses, and could include landscape features, screening devices (including for rooftop and other mechanical equipment), and careful consideration of the placement of windows, patios and roof terraces.
- v. Effective buffering techniques between new development and existing development on adjacent properties will be employed.
- vi. Best practices to improve accessibility will be incorporated into building design to facilitate people of all ages to fully participate in community life.
- vii. Buildings and private landscape areas adjacent to the Fraser River and Como Creek will be designed to provide a gentle transition from areas supporting intensive people-related uses to habitat areas.

4.2.1 Waterfront Village Development Permit Area - *continued*

- viii. Residential buildings shall be designed to express variety and individuality through varied massing, roof forms and material selection. Some higher density development parcels will integrate townhouses at grade, with either direct street access or communal access, in combination with high rise towers.
- ix. Light Industrial/Business Park buildings will pay special attention to the long-term flexibility of use for these buildings, particularly their ability to house either large-scale operations or a variety of smaller uses. Buildings fronting streets and sidewalks within the public Right-of-Way shall encourage a built form that is human scaled. Emphasis will be placed on reducing the visual bulk of larger buildings and providing visual interest through variations in massing and roof forms.
- x. Buildings along King Edward Street and adjacent to street ends that terminate at the Fraser River shall encourage the integration of heritage works at key nodes as identified in the Heritage Interpretive Program – Schedule F. Heritage works may take the form of free standing objects integrated into the landscape or be incorporated into the architectural façade of a building (such as murals or screens).
- xi. Buildings along the Main Street will contribute to a comfortable public realm and create a street wall that defines and encloses the street, with texture and articulation used to create visual interest and variety. Built form will maximize exposure to the sun and allow stores, cafés and restaurants to spill out onto sidewalks to enhance the pedestrian experience along this street and draw people from United Boulevard to the Fraser River.
- xii. Buildings adjacent to the privately owned publicly accessible portion of the Main Retail Street will be designed to define an Urban Plaza that culminates in a Pier or other similar structure adjacent to the Fraser River.
- xiii. Buildings adjacent to Shared Streets will be important character areas for the neighbourhood. Shared streets maximize the available space to all street users and encourage the integration of pedestrians, bicyclists, parked and vehicles. The integration of the private and public realms adjacent to these streets should differ from other neighbourhood streets in scale and materials.

4.2.1 Waterfront Village Development Permit Area - *continued*

- xiv.** Commercial buildings shall promote pedestrian-orientation and provide a disciplined rhythm of storefronts with emphasis placed on smaller scale storefronts where appropriate. Frontage should wrap the corner of buildings at street intersections and extend along side streets so as to minimize the potential for blank walls.
- xv.** All commercial or mixed use building frontages will provide continuous weather protection. Weather protection should primarily be glass to maximize natural light at street level and should be uninterrupted to provide constant protection from the elements.
- xvi.** The Private Recreation, Institutional and other accessory public buildings along the Fraser River shall be distinct from other private buildings, oriented to the waterfront, and use variation in massing and material selection.
- xvii.** The Central Energy Plant will be designed to integrate with the adjacent built form.
- xviii.** Signage shall be well integrated into the design of buildings. Scale and proportion will be considered to ensure that signs do not become a dominant feature of a building façade.
- xix.** Materials should be durable and reflect the history of the Fraser Mills site, the location, and heritage of the area. Materials such as exposed timbers, stone, brick, concrete, architectural wood, pre-cast and metal panels are appropriate. Where possible, emphasis should be placed on the use of glass to maximize natural illumination within the building and to animate the adjacent streets, parks, sidewalks and trail.
- xx.** Off street surface parking lots shall include landscape areas, in addition to perimeter landscaping, to provide shade and break up the impervious surfaces.
- xxi.** Interim measures are to be provided, such as landscaping, appropriate fencing, façades and site signage, to enhance the pedestrian environment and communicate the development plans for subsequent phases.
- xxii.** Crime Prevention Through Environmental Design (CPTED) guidelines will be followed in the siting and configuration of new development sites and building design.
- xxiii.** Development variances may be considered by Council within this Development Permit Area.

4.3 Windsor Gate

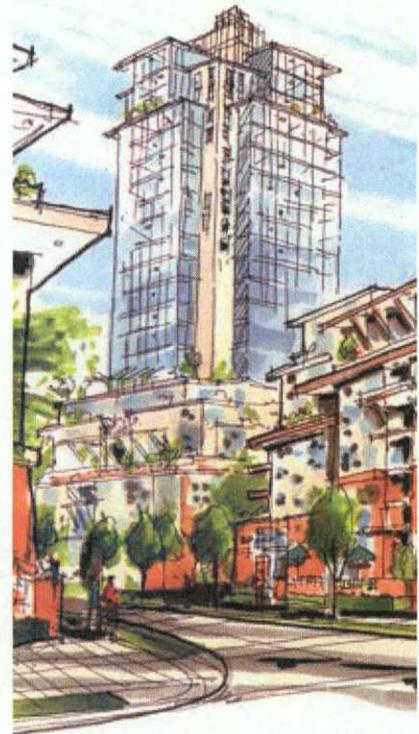
4.3.1 WINDSOR GATE DEVELOPMENT PERMIT AREA

The guidelines in this section apply to all types of development within the Windsor Gate Development Permit Area as shown on Schedule B.

For further information on the Windsor Gate development, including the Master Plan Organization, General Design Considerations and Streetscape Standards please refer to the *Windsor Gate Design Guidelines*.

a) Unifying Elements

- A common design theme should unify the first storey level throughout the development. This will be achieved through the use of building design elements, choice of materials such as masonry, lighting, street entrances, roof ridges and eave line designs, finishes, or colours. Areas where this expression should be found include:
 - i. Principal residential entries to multi-family buildings will all be required to have suitable weather protection such as canopies, port cocheres etc.
 - ii. Principle building entries will be clad with brick, stone or architectural concrete;
 - iii. City home entries will be clearly identifiable and have a similar door and hardware design and will be clad with brick, stone or architectural concrete;
 - iv. Brick or stone will occur in the pedestrian realm in public areas;
 - v. Steel and glass canopies will be used at the retail street frontage;
 - vi. Contemporary fenestration with minimal mullion detailing will be incorporated in all buildings;
 - vii. Low rise buildings (and townhomes) will have pitched roofs with substantial overhangs (typical 2'-6");
 - viii. Cornice lines will be incorporated at varying heights to enhance the massing of the mid-rise and high-rise buildings;
 - ix. High rise buildings shall have flat roofs; which are articulated along the edge with cornice detailing.



4.3.1 a Unifying Design Elements

4.3.1 Windsor Gate Development Permit Area - continued

b) Public Access to Private Lands

- Where a private amenity has been created on a street front, it would be appropriate to allow public access to such space if it is seen to be an extension of the pedestrian environment – provided adequate security has been provided for the adjacent private space. These semi-public open spaces should be urban in character, with urban standard planting and street furniture (benches, waste bins, lighting).



4.3.1 b Semi-Public Design

c) Treatment of Street Setbacks

- For Nakoma Place and Windsor Gate where a minimum setback is desired, city home frontages should be a combination of concrete or patio pavers, concrete stair and porch, planters and landscaping, and may include retaining walls of a maximum individual height of 1.0 metre and a visually permeable fence of no more than 1.2 metre high. Setbacks on other municipal streets shall be landscaped, fenced or walled. Wall materials where exposed to public view may be articulated architectural concrete, brick/ stone faced or decorative concrete unit block e.g. Pisa or Allen Block. Walls not exposed to public view in the rear yards may be constructed of timber cribbing.



4.3.1 c Public to Private Setback

d) Above-Ground Parking Walls

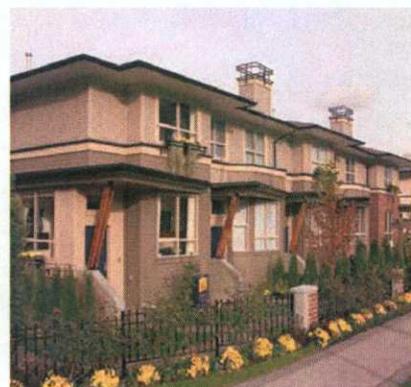
- In situations where concrete parking garage walls higher than 1.0 metre are exposed to the street, setbacks may be a combination of terraced, planted and lighted retaining walls – each portion of which shall be no higher than 1.0 metre, or may be articulated architectural concrete.



4.3.1 d Exposed Foundation Walls

e) Townhouse Orientation

- i. Townhomes situated along Kensal Walk shall have their entrances along this frontage with garage entrances accessible from an auto-lane behind the buildings.
- ii. When garages are situated on the same elevation of a townhouse building as the unit entry, architectural elements such as roof overhangs, balconies, and trellis shall be incorporated to minimize the visual impacts of the overhead garage doors.



4.3.1 e Townhouse Orientation

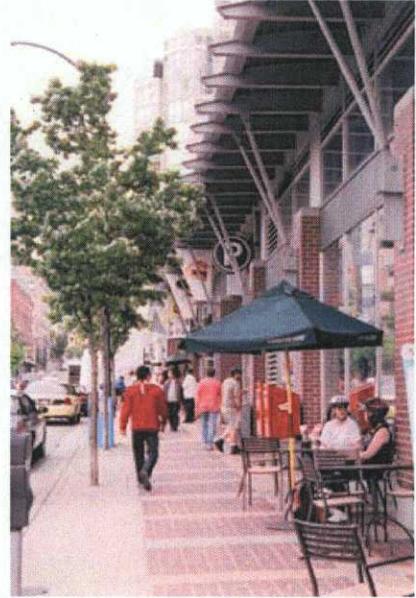
4.3.1 Windsor Gate Development Permit Area - *continued*

f) Roof Forms

- Sloped roofs shall be incorporated within the low-rise buildings.

g) Materials

- i. For high-rise buildings materials should be selected from a high quality urban palette, including concrete, brick or stone masonry, acrylic stucco, glass, steel and other metal panels.
- ii. For low-rise buildings and the podium portion of high-rises materials should include: brick or stone accents, acrylic stucco or Hardi-plank, concrete, and metal architectural, and wood siding above the first floor on the frontage streets. Decorative metal frontage fences or masonry walls should be employed to unify the streetscape.
- iii. Materials selected for use on the townhomes should include natural materials such as wood trim and shingles, durable, ecologically friendly materials such as vinyl siding and also brick and metal appointments to offer continuity with the overall appearance of the community.



4.3.1 g High-Quality Urban Materials

4.4 Partington Creek

4.4.1 PARTINGTON CREEK NEIGHBOURHOOD CENTRE DEVELOPMENT PERMIT AREA

The guidelines in this section apply to all types of development within the Partington Creek Neighbourhood Centre Development Permit Area as shown on Schedule "B".

a) Guidelines

- i. Buildings should be street-fronting with active frontages. (i.e.: entrances, abundant windows, front porches, ground-oriented residential and commercial units) that directly address the street.
- ii. Buildings fronting onto Commercial Street Frontage should contribute to a pedestrian-friendly public realm and create a streetwall that defines and encloses the street. Building forms should maximize solar exposure and provide opportunities for stores and restaurants to spill out onto the sidewalk.
- iii. Consider the views of adjacent buildings and buildings further up the slope when designing and siting new developments.
- iv. All on-site parking should be located underground or concealed within a building. Where underground or concealed parking is not technically feasible it should be located away from the street frontage, at the rear or side of the building. On-street parking in front of buildings may be supported, where feasible.

DRAFT

Environmental Guidelines



The Guidelines in this section apply to all types of development in all Development Permit Areas for the protection of the natural environment and the protection of development from natural hazards.

5.1 Watercourse Protection

5.1.1 WATERCOURSE PROTECTION DEVELOPMENT PERMIT AREA

The Watercourse Protection Development Permit area is to ensure the review of proposed development activities adjacent to watercourses, in accordance with the *Zoning Bylaw's* Riparian Area Regulation (RAR) provisions.

Further watercourses may be identified as development plans are undertaken. Fish habitat values, top-of-bank surveys and biophysical and hydrological characteristics of all watercourses will need to be evaluated by the applicant prior to the issuance of a Development Permit. Streamside protection measures will be determined, as appropriate, in consultation with senior government regulatory agencies. Formal authorization under the Federal *Fisheries Act* may also be necessary.

a) Guidelines

- i. Development within 50 metres of the top-of-bank of a watercourse located within the Northeast Coquitlam Area Plan and Development within 30 metres of the top-of-bank of a watercourse located within the Austin Heights, Maillardville, and **Burquitlam-Lougheed** Neighbourhood Plans triggers a Watercourse Protection Development Permit and a review under the Riparian Areas Regulation (RAR) provisions of the *Zoning Bylaw*.
- ii. The Watercourse Protection Development Permit shall set conditions and may also facilitate modification of the applicable regulations to further achieve the enhancement and protection of watercourses, while ensuring the following guidelines are met:
 - That environmentally sensitive areas and features are identified, protected, restored, replaced or enhanced, as appropriate.
 - That the quality of water and rate of runoff to receiving watercourses shall be maintained as close to pre-application conditions as possible, or improved.
 - That the proposed alteration of land or development will not result in erosion, sloughing, landslip or flooding.
 - Riparian areas along watercourses shall be protected in accordance with standards which Council may approve by bylaw and through any necessary authorization by senior levels of government.

5.1.2 SCOTT CREEK DEVELOPMENT PERMIT AREA

The guidelines in this section apply to all types of development within the Scott Creek Development Permit Area as shown on Schedule A.

a) Guidelines

- i. Provide temporary fencing, sediment control, signage and such other measures as may be required to ensure that construction activity does not impinge on fisheries habitat areas along Scott Creek and the Coquitlam River during construction within this Development Permit Area.
- ii. Conduct site grading, site drainage and the siting of buildings and parking areas in a manner that does not adversely affect fisheries values in the adjacent streams.

5.2 Braid Street Fill Site

5.2.1 BRAID STREET FILL SITE DEVELOPMENT PERMIT AREA

The guidelines in this section apply to all types of development within the Braid Street Fill Site Development Permit Area as shown on Schedule A.

This area is a former landfill site and is subject to potential hazards, including differential land settlement, explosive and noxious gases, noxious leachates and unpleasant odours.

a) Guidelines

- i. Provide a report certified by a professional engineer with demonstrated expertise in geotechnical matters and the handling of dangerous gases and leachates where potential hazards exist at the applicant's expense to assist Council in determining detailed guidelines and conditions for issuance of such a permit.
- ii. Provide siting and servicing of development to allow for construction of the United-Braid connector.
- iii. Complete remediation for the site and redesignation to an industrial land use before new development proceeds.

5.3 Hazard Risk Management

5.3.1 UNSTABLE SLOPES DEVELOPMENT PERMIT AREA

The guidelines in this section apply to all types of development within the Unstable Slopes Development Permit Area as shown on Schedule B.

This Development Permit Area has been created because development of these lands involved extensive earthworks to reshape former gravel pits, adjacent to a tributary which lies just above a section of Hoy Creek that has high fisheries habitat values. Under these circumstances, there are particular possibilities of hazard or damage to the public, property, or the environment, from land slippage, erosion, flooding, or the discharge of mud or silt.

a) Guidelines

- i. Submission of a geotechnical report by a qualified professional engineer;
- ii. Supervision of excavation or placement of fill by a qualified professional engineer;
- iii. Variance of the siting requirements of the Zoning Bylaw for structures and parking areas;
- iv. Variance or supplementing of the requirements of the City of Coquitlam Subdivision and Development Servicing Bylaw for drainage works, earth-retaining works or revegetation;
- v. Retention of existing vegetation;
- vi. Enactment of "specified areas" Bylaw to provide for maintenance of any steep slopes which are proposed to be transferred to City ownership; together with registration against adjacent lots of a "save harmless" covenant in favour of the City;
- vii. Adherence to a specified phasing of development;
- viii. Detailed lot grading plans;
- ix. Deposit of securities to ensure that contravention of a condition of a permit does not result in unsafe conditions;

5.3.1 Unstable Slopes Development Permit Area - *continued*

- x. Design review of buildings other than for single-family residential use to assure that the siting and design of such buildings is appropriate to site conditions;
- xi. A survey plan prepared by a certified BC land surveyor showing the top-of-bank and natural boundary of streams relative to legal boundaries;¹
- xii. An environmental inventory and impact assessment related to water courses by a registered professional biologist (to measure compliance with the federal Fisheries Act), and any other appropriate environmental legislation.

5.3.2 INTERFACE WILDFIRE RISK MANAGEMENT DEVELOPMENT PERMIT AREA

The guidelines in this section apply to all types of development within the Interface Wildfire Risk Management Development Permit Area as shown on Schedule "B".

The lands identified as the Interface Wildfire Risk Management Development Permit Area are designated a hazardous condition development permit area to help protect buildings and properties near interface boundaries from heat radiation, direct flame contact and/or airborne embers produced by interface wildfires.

For the purposes of this section, flammable vegetation is defined as coniferous trees and shrubs (mature and immature fir, cedar, pine, spruce, juniper, etc.) and significant concentrations of ground fuels (deadfall, long grasses), and as determined by City staff.

Low/limited flammability vegetation is defined as deciduous trees and shrubs and other low/limited flammability vegetation, and as determined by City staff.

a) Buildings and Properties Directly Adjacent to Interface Boundary

- i. Require Class 'A' asphalt roofing as a minimum standard in new construction.
- ii. Require non-combustible siding (i.e., hardi-plank) on building façades that directly face interface areas.

5.3.2 Interface Wildfire Risk Management Development Permit Area - *continued*

- iii. Require heavy timber construction, fire-retardant treated materials and/or other non-flammable materials for decks and railings that directly face interface areas.
- iv. Wherever possible, pursue opportunities for large setbacks (10 metres minimum) between new buildings and forested areas (urban-interface boundary).

b) Buildings and Properties Located Within 200 Metres of Interface Boundary (but not directly adjacent to interface boundary)

- i. Require the use of limited/non-combustible roofing materials (Class 'A' or 'B').

c) Landscaping for Properties Directly Adjacent to Interface Boundary

- i. Remove flammable vegetation, in yards between new buildings and forested areas (urban-interface boundary), prior to occupancy, subject to environmental/riparian area requirements.
- ii. Require new vegetation/planting in yards between buildings and forested areas (urban-interface boundary) to be of a low/limited flammability variety, adequately spaced (trees located 3-6 metres apart) and not overhanging building roofs and decks.
- iii. Any landscaping undertaken must ensure that environmentally sensitive areas are protected in accordance with City bylaws.
- iv. The use of sprinkler irrigation systems in yards between buildings and forested areas is encouraged.

d) Procedures

For properties directly adjacent to interface boundaries, developers and builders are required to either:

- i. Apply the proceeded Development Permit Area guidelines, or
- ii. Retain a Registered Professional Forester, Registered Forest Technician or Registered Professional Engineer qualified by training or experience in fire protection to conduct a fuel hazard assessment and develop recommendations for mitigating these hazards, and apply these recommendations through development of these properties, subject to review and concurrence by City staff.

Glossary

- a) **Architecturally Treated Concrete** is concrete that has been, scored, stamped, shaped, coloured, polished and/or treated in an architecturally interesting manner to make it more aesthetically pleasing.
- b) **Employment Living Street Frontage** is a street where the ground level units must be designed to accommodate either live/work, commercial and/or residential uses.
- c) **Frontage Zone** is the portion of the sidewalk that is directly adjacent to the front of a building. This is often the location for cafe tables and outdoor display. The frontage zone can be part of the public right-of-way or located on private property.
- d) **Furnishing Zone** is the portion of a sidewalk that is directly adjacent to the curb. It is the location for street trees, street lights and other street furniture. This zone acts as a buffer between the street and the pedestrian through zone. This area is often called a boulevard.
- e) **High-rise Building** is a building that is generally above twelve storeys in height. Also referred to as a tower.
- f) **Interlocking Landscaping Bricks** are small scale bricks, often made of concrete, that can stack and fit together to form a retaining wall. The exposed face is treated in an aesthetically pleasing manner. This does not include large scale "lock-blocks".
- g) **Landmark Building** a building deemed to have a high-degree of architectural design excellence and located on a prominent and/or 'gateway' site.
- h) **Landscaped Parking** is a surface parking lot which contains a number of, evenly distributed, landscaped areas containing trees and other plant material, that is used to break up the parking lot into smaller sections.
- i) **Low-rise Building** is a building that is generally less than four storeys in height.
- j) **Mandatory Commercial Street Frontage** means a street frontage along which development must provide continuous commercial storefront units. Appropriate breaks in the frontage may be considered for access to upper level uses, underground parking or a urban plaza that complements the commercial street.

- k) **Mid-rise Building** is a building that is generally between four to twelve storeys in height.
- l) **Optional Commercial Street Frontage** means a street frontage along which development will be encouraged to provide continuous commercial storefront units. Appropriate breaks in the continuous commercial frontage may be considered for access to upper level uses, underground parking or a urban plaza that complements the commercial street.
- m) **Pedestrian Through Zone** is the portion of the sidewalk where the majority of walking occurs. This area shall be kept free of obstacles to allow for unobstructed pedestrian movement.
- n) **Permeable Paving** is a type of surface material which allows water to penetrate into the ground below.
- o) **Public Realm** is all parts of the city which are publicly accessible. It includes all streets, walkways, squares, parks and open spaces as well as semi-public spaces.
- p) **Residential Street Frontage** means a street frontage along which development shall provide continuous, ground-oriented, residential units. Appropriate breaks in the continuous residential frontage may be considered for access to upper level uses, underground parking or a public space that complements the residential street.
- q) **Secondary Active Street Frontage** means a street frontage along which development shall provide active, ground-oriented commercial, residential and/or employment living uses. Appropriate breaks in the continuous frontage may be considered for access to upper level uses, underground parking or a public space that complements the streetscape.
- r) **Street Enclosure** is the relationship of a street to the buildings along it and is measured by the ratio of the distance between buildings to the building height.

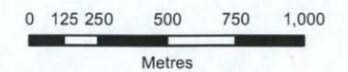
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Coquitlam

SCHEDULE A DEVELOPMENT PERMIT AREAS SOUTHWEST COQUITLAM

Legend

- ALLARD - LEBLEU
- AUSTIN HEIGHTS NEIGHBOURHOOD CENTRE
- BRAID STREET FILL SITE
- BURQUITLAM - LOUGHEED NEIGHBOURHOOD CENTRE
- LAVAL SQUARE
- MAILLARDVILLE NEIGHBOURHOOD CENTRE
- SCOTT CREEK
- WATERFRONT VILLAGE CENTRE



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Map Projection: UTM Nad1983 10N
Prepared By: Planning & Development
Source: City of Coquitlam - OCP GIS layer

